

## **Schedules**

## **SCHEDULE - A**

*(See Clauses 2.1 and 8.1)*

### **SITE OF THE PROJECT**

1. [

#### **2. The Site**

- 21 Single/Intermediate lane shall include the land, buildings, structures and road works as described in Annex-I of this Schedule-A.
- 22 The dates of handing over Right of Way to the Contractor are specified in the Annex-II of this Schedule A.
- 23 An inventory of the Site including the land, buildings, structures, road works, trees and any other immovable property on, or attached to, the Site shall be prepared jointly by the Authority Representative and the Contractor, and such inventory shall form part of the memorandum referred to in Clause 8.2.1 of this Agreement.
- 24 The alignment plans of the Project Highway are specified in Annex-III. In the case of sections where no modification in the existing alignment of the Project Highway is contemplated, the alignment plan has not been provided. Alignment plans have only been given for sections where the existing alignment is proposed to be upgraded. The proposed profile of the Project Highways shall be followed by the contractor with minimum FRL as indicated in the alignment plan. The contractor, however, improve/upgrade the Road Profile as indicated in Annexure-III based on site/design requirement.
- 25 The status of the environment clearances obtained or awaited is given in Annex - IV.

## Annexure - I

(Schedule-A)

### Site

Note: Through suitable drawings and description in words, the land, buildings, structures and road works comprising the Site shall be specified briefly but precisely in this Annex-I. All the chainages/location referred to in Annex-I to Schedule A shall be existing chainages.

#### 1. Site

The Site of the single/intermediate lane Project Highway comprises the section of National Highway – 4 after Ferrargunj at Km 45.0 to Jirkatang at Km 59.2 in the Union Territory of Andaman & Nicobar Islands. The land, carriageway and structures comprises the Site are described below.

#### 2. Land

The Site of the Project Highway comprises the land (sum total of land already in possession and land to be possessed) as described below:

#### Land Details

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
1	45.00	7.18	6.78	13.95	
2	45.03	8.13	7.28	15.40	
3	45.06	9.25	9.25	18.50	
4	45.09	9.90	8.80	18.70	
5	45.12	9.85	6.98	16.83	
6	45.15	8.60	7.10	15.70	
7	45.18	8.55	8.15	16.70	
8	45.21	7.90	8.00	15.90	
9	45.24	8.65	8.85	17.50	
10	45.27	7.55	8.35	15.90	
11	45.3	7.80	9.00	16.80	
12	45.33	8.95	8.75	17.70	
13	45.36	9.25	5.55	14.80	
14	45.39	11.40	5.70	17.10	
15	45.42	11.00	5.70	16.70	
16	45.45	8.10	10.00	18.10	
17	45.48	6.75	11.05	17.80	
18	45.51	7.80	12.80	20.60	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
19	45.54	11.30	11.70	23.00	
20	45.57	11.20	11.60	22.80	
21	45.6	10.50	10.10	20.60	
22	45.63	12.40	11.10	23.50	
23	45.66	6.40	11.00	17.40	
24	45.69	5.80	11.90	17.70	
25	45.72	11.45	10.85	22.30	
26	45.75	12.60	7.20	19.80	
27	45.78	5.90	8.70	14.60	
28	45.81	9.10	8.70	17.80	
29	45.84	9.23	8.23	17.45	
30	45.87	11.60	8.10	19.70	
31	45.9	7.75	8.15	15.90	
32	45.93	7.85	6.55	14.40	
33	45.96	12.93	6.03	18.95	
34	45.99	12.05	8.15	20.20	
35	46.02	7.95	9.75	17.70	
36	46.05	13.95	8.65	22.60	
37	46.08	12.05	14.15	26.20	
38	46.11	8.25	16.35	24.60	
39	46.13	9.65	15.65	25.30	
40	46.16	11.70	11.10	22.80	
41	46.19	12.80	10.60	23.40	
42	46.22	9.80	10.30	20.10	
43	46.25	13.03	8.83	21.85	
44	46.28	9.70	13.90	23.60	
45	46.31	13.75	12.85	26.60	
46	46.34	14.70	9.50	24.20	
47	46.37	7.80	9.80	17.60	
48	46.4	11.80	9.00	20.80	
49	46.43	11.70	8.50	20.20	
50	46.46	14.85	6.75	21.60	
51	46.49	7.65	10.55	18.20	
52	46.52	9.00	10.90	19.90	
53	46.55	9.60	9.60	19.20	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
54	46.58	8.20	9.10	17.30	
55	46.61	9.45	10.95	20.40	
56	46.64	8.80	8.50	17.30	
57	46.7	13.00	11.00	24.00	
58	46.73	10.05	7.95	18.00	
59	46.76	9.10	10.00	19.10	
60	46.79	7.90	10.30	18.20	
61	46.82	11.40	10.30	21.70	
62	46.85	11.80	10.00	21.80	
63	46.88	12.00	10.20	22.20	
64	46.91	14.30	10.20	24.50	
65	46.94	7.20	15.70	22.90	
66	46.97	11.25	11.95	23.20	
67	47.00	8.80	13.80	22.60	
68	47.03	8.25	11.95	20.20	
69	47.06	9.20	8.10	17.30	
70	47.09	9.80	6.00	15.80	
71	47.12	12.05	5.55	17.60	
72	47.15	17.90	6.80	24.70	
73	47.18	14.00	8.80	22.80	
74	47.21	10.65	10.95	21.60	
75	47.24	9.30	10.90	20.20	
76	47.27	7.35	9.75	17.10	
77	47.3	15.00	6.10	21.10	
78	47.33	10.45	6.75	17.20	
79	47.36	9.25	7.05	16.30	
80	47.39	14.30	5.90	20.20	
81	47.42	8.10	10.10	18.20	
82	47.45	5.15	10.25	15.40	
83	47.48	5.25	10.05	15.30	
84	47.51	4.75	10.75	15.50	
85	47.54	4.25	8.85	13.10	
86	47.57	4.60	9.30	13.90	
87	47.6	7.00	8.30	15.30	
88	47.63	5.35	15.75	21.10	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
89	47.66	5.55	13.95	19.50	
90	47.69	4.95	16.55	21.50	
91	47.72	10.20	16.30	26.50	
92	47.75	12.65	12.95	25.60	
93	47.78	14.00	8.10	22.10	
94	47.81	13.70	8.00	21.70	
95	47.84	15.10	6.50	21.60	
96	47.87	14.80	11.80	26.60	
97	47.9	10.00	11.90	21.90	
98	47.93	13.45	10.15	23.60	
99	47.96	11.45	9.25	20.70	
100	47.99	16.25	11.65	27.90	
101	48.02	11.80	10.70	22.50	
102	48.05	11.90	9.80	21.70	
103	48.08	11.20	9.80	21.00	
104	48.11	11.75	9.55	21.30	
105	48.14	12.80	7.90	20.70	
106	48.17	14.40	9.90	24.30	
107	48.2	13.75	7.35	21.10	
108	48.23	12.55	9.45	22.00	
109	48.26	11.30	10.60	21.90	
110	48.29	11.20	12.00	23.20	
111	48.32	11.25	10.75	22.00	
112	48.35	12.25	9.55	21.80	
113	48.38	11.35	12.05	23.40	
114	48.41	9.70	2.15	11.85	
115	48.44	12.35	9.55	21.90	
116	48.47	14.15	8.15	22.30	
117	48.5	9.60	8.70	18.30	
118	48.53	13.80	11.80	25.60	
119	48.56	13.75	9.65	23.40	
120	48.59	15.35	14.05	29.40	
121	48.62	15.90	9.20	25.10	
122	48.65	16.40	8.50	24.90	
123	48.68	17.90	4.80	22.70	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
124	48.71	14.35	9.65	24.00	
125	48.74	13.90	8.30	22.20	
126	48.77	13.75	10.15	23.90	
127	48.8	7.05	9.65	16.70	
128	48.83	4.80	8.70	13.50	
129	48.86	5.25	9.75	15.00	
130	48.89	5.65	9.45	15.10	
131	48.92	8.25	6.95	15.20	
132	48.95	8.65	8.05	16.70	
133	48.98	11.80	5.40	17.20	
134	49.01	12.60	6.70	19.30	
135	49.04	11.00	9.00	20.00	
136	49.07	11.75	9.75	21.50	
137	49.1	12.85	9.65	22.50	
138	49.12	12.90	11.00	23.90	
139	49.15	11.75	18.65	30.40	
140	49.18	11.40	15.40	26.80	
141	49.21	12.25	9.75	22.00	
142	49.24	10.30	12.10	22.40	
143	49.27	12.95	11.15	24.10	
144	49.3	14.25	11.75	26.00	
145	49.33	10.95	12.55	23.50	
146	49.36	7.30	10.50	17.80	
147	49.39	10.00	9.60	19.60	
148	49.45	10.20	11.90	22.10	
149	49.48	9.50	12.00	21.50	
150	49.51	14.00	7.80	21.80	
151	49.54	12.95	8.15	21.10	
152	49.57	8.75	10.85	19.60	
153	49.6	5.60	10.10	15.70	
154	49.63	5.65	13.85	19.50	
155	49.66	6.10	12.70	18.80	
156	49.69	7.90	10.90	18.80	
157	49.72	6.75	15.75	22.50	
158	49.75	8.35	14.35	22.70	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
159	49.78	6.90	14.60	21.50	
160	49.81	9.45	14.05	23.50	
161	49.84	8.00	15.00	23.00	
162	49.87	11.40	10.20	21.60	
163	49.90	8.25	11.95	20.20	
164	49.93	9.05	10.25	19.30	
165	49.96	11.45	12.65	24.10	
166	49.99	10.05	10.55	20.60	
167	50.02	10.55	9.85	20.40	
168	50.05	15.20	7.40	22.60	
169	50.08	16.65	5.65	22.30	
170	50.11	17.45	5.45	22.90	
171	50.14	19.10	6.39	25.49	
172	50.17	12.70	13.60	26.30	
173	50.2	10.30	9.20	19.50	
174	50.23	11.00	10.90	21.90	
175	50.26	8.25	19.15	27.40	
176	50.29	8.50	17.70	26.20	
177	50.31	10.75	13.25	24.00	
178	50.34	15.25	7.25	22.50	
179	50.37	11.70	9.00	20.70	
180	50.4	10.80	12.20	23.00	
181	50.43	10.70	13.90	24.60	
182	50.46	11.40	10.80	22.20	
183	50.49	11.05	9.85	20.90	
184	50.52	11.90	10.90	22.80	
185	50.55	11.85	10.25	22.10	
186	50.58	8.45	12.85	21.30	
187	50.61	12.30	9.80	22.10	
188	50.64	13.15	9.05	22.20	
189	50.67	13.90	6.40	20.30	
190	50.7	12.95	10.35	23.30	
191	50.73	13.15	7.15	20.30	
192	50.76	10.00	13.20	23.20	
193	50.79	11.10	8.30	19.40	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
194	50.82	12.75	10.75	23.50	
195	50.85	7.60	5.80	13.40	
196	50.88	14.70	6.70	21.40	
197	50.91	15.90	6.90	22.80	
198	50.94	15.60	7.60	23.20	
199	50.99	12.20	8.50	20.70	
200	51.02	10.05	13.25	23.30	
201	51.05	9.15	10.65	19.80	
202	51.08	16.85	6.85	23.70	
203	51.11	16.35	8.15	24.50	
204	51.14	16.25	7.55	23.80	
205	51.17	12.85	9.05	21.90	
206	51.2	11.60	9.40	21.00	
207	51.23	10.45	10.95	21.40	
210	51.26	11.05	13.05	24.10	
211	51.29	12.60	11.90	24.50	
212	51.32	13.60	11.60	25.20	
213	51.35	11.60	7.70	19.30	
214	51.38	12.05	9.15	21.20	
215	51.41	11.90	9.90	21.80	
216	51.44	12.70	9.70	22.40	
217	51.47	12.40	11.60	24.00	
218	51.5	7.70	7.90	15.60	
219	51.53	12.00	10.50	22.50	
220	51.56	9.55	9.65	19.20	
221	51.59	8.35	9.65	18.00	
222	51.62	11.70	7.70	19.40	
223	51.65	11.45	11.15	22.60	
224	51.68	10.10	10.50	20.60	
225	51.71	12.30	8.70	21.00	
226	51.74	15.30	3.80	19.10	
227	51.77	15.15	8.25	23.40	
228	51.8	12.75	9.75	22.50	
229	51.83	17.40	11.60	29.00	
230	51.86	21.60	8.90	30.50	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
231	51.89	13.25	10.75	24.00	
232	51.92	13.75	13.04	26.79	
233	51.95	16.70	10.80	27.50	
234	51.98	17.95	8.75	26.70	
235	52.03	10.50	7.00	17.50	
236	52.04	11.55	8.15	19.70	
237	52.07	11.25	4.25	15.50	
238	52.1	11.70	3.80	15.50	
239	52.13	10.45	3.85	14.30	
240	52.16	8.40	3.60	12.00	
241	52.19	8.00	4.40	12.40	
242	52.22	7.80	4.20	12.00	
243	52.25	8.15	5.55	13.70	
244	52.28	7.00	4.40	11.40	
245	52.31	9.10	4.50	13.60	
246	52.34	4.75	4.65	9.40	
247	52.37	5.55	6.25	11.80	
248	52.4	5.45	4.95	10.40	
249	52.43	7.50	4.40	11.90	
250	52.46	8.05	4.25	12.30	
251	52.49	7.95	6.05	14.00	
252	52.52	6.35	5.15	11.50	
253	52.55	6.25	5.25	11.50	
254	52.58	5.90	5.20	11.10	
255	52.61	6.00	5.20	11.20	
256	52.64	5.30	5.30	10.60	
257	52.67	5.00	4.30	9.30	
258	52.70	5.40	4.50	9.90	
259	52.73	5.10	5.20	10.30	
260	52.76	9.00	4.60	13.60	
261	52.79	7.00	4.20	11.20	
262	52.81	6.30	4.40	10.70	
263	52.84	5.75	5.75	11.50	
264	52.87	7.75	3.85	11.60	
265	52.90	7.95	4.35	12.30	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
266	52.93	7.45	7.35	14.80	
267	52.96	5.65	6.95	12.60	
268	52.99	7.40	4.40	11.80	
269	53.02	5.65	4.45	10.10	
270	53.05	5.75	3.75	9.50	
271	53.08	6.00	4.00	10.00	
272	53.11	5.20	5.30	10.50	
273	53.14	5.10	5.60	10.70	
274	53.17	6.00	5.20	11.20	
275	53.20	5.30	4.40	9.70	
276	53.23	4.30	5.10	9.40	
277	53.26	7.40	5.20	12.60	
278	53.29	6.35	5.15	11.50	
279	53.32	4.90	6.00	10.90	
280	53.35	7.25	3.85	11.10	
281	53.38	10.80	4.30	15.10	
282	53.41	8.05	5.85	13.90	
283	53.44	5.90	3.90	9.80	
284	53.50	4.95	3.85	8.80	
285	53.53	5.40	3.70	9.10	
286	53.56	5.80	5.60	11.40	
287	53.59	9.40	4.60	14.00	
288	53.62	5.60	5.30	10.90	
289	53.65	5.45	4.05	9.50	
290	53.68	4.90	4.90	9.80	
291	53.71	5.35	4.75	10.10	
292	53.74	6.00	5.00	11.00	
293	53.77	5.80	4.40	10.20	
294	53.8	5.00	4.80	9.80	
295	53.83	4.95	4.15	9.10	
296	53.86	5.60	3.70	9.30	
297	53.89	4.10	4.00	8.10	
298	53.92	5.00	4.40	9.40	
299	53.95	5.40	4.80	10.20	
300	53.98	4.60	4.40	9.00	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
301	54.01	5.10	3.50	8.60	
302	54.04	5.45	4.95	10.40	
303	54.07	5.50	3.60	9.10	
304	54.1	4.75	4.05	8.80	
305	54.13	3.90	4.30	8.20	
306	54.16	5.00	4.20	9.20	
307	54.19	4.95	4.05	9.00	
308	54.22	5.15	3.85	9.00	
309	54.25	4.65	4.25	8.90	
310	54.28	4.30	3.60	7.90	
311	54.31	5.05	4.85	9.90	
312	54.34	4.55	5.05	9.60	
313	54.37	5.75	4.75	10.50	
314	54.40	5.05	6.05	11.10	
315	54.43	4.75	5.25	10.00	
316	54.46	4.95	5.45	10.40	
317	54.49	4.25	5.95	10.20	
318	54.52	4.20	6.40	10.60	
319	54.55	4.15	5.75	9.90	
320	54.58	4.85	4.75	9.60	
321	54.61	7.05	5.55	12.60	
322	54.64	8.80	6.80	15.60	
323	54.67	7.95	7.65	15.60	
324	54.7	5.60	7.50	13.10	
325	54.73	7.90	5.70	13.60	
326	54.76	4.75	5.05	9.80	
327	54.79	4.15	5.55	9.70	
328	54.82	5.80	5.80	11.60	
329	54.85	8.35	5.25	13.60	
330	54.88	7.20	4.90	12.10	
331	54.91	9.85	5.75	15.60	
332	54.94	5.65	6.25	11.90	
333	54.97	6.75	6.45	13.20	
334	55.00	8.45	6.35	14.80	
335	55.03	7.40	4.20	11.60	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
336	55.06	7.10	6.30	13.40	
337	55.09	8.85	6.85	15.70	
338	55.12	8.50	6.40	14.90	
339	55.15	6.55	6.45	13.00	
340	55.18	6.30	6.60	12.90	
341	55.21	9.85	6.95	16.80	
342	55.24	7.15	6.95	14.10	
343	55.27	6.05	6.75	12.80	
344	55.30	6.50	6.60	13.10	
345	55.33	7.15	6.35	13.50	
346	55.36	6.50	6.30	12.80	
347	55.39	5.55	5.75	11.30	
348	55.42	6.65	5.55	12.20	
349	55.45	6.30	6.20	12.50	
350	55.48	7.05	6.85	13.90	
351	55.51	7.05	7.05	14.10	
352	55.54	4.95	6.35	11.30	
353	55.57	5.90	6.20	12.10	
354	55.60	6.75	6.95	13.70	
355	55.63	6.15	6.95	13.10	
356	55.66	5.45	5.75	11.20	
357	55.69	5.80	5.10	10.90	
358	55.72	6.45	5.85	12.30	
359	55.75	6.35	5.55	11.90	
360	55.78	7.10	6.70	13.80	
361	55.81	6.45	7.25	13.70	
362	55.84	7.20	9.00	16.20	
363	55.87	8.20	6.40	14.60	
364	55.90	6.35	7.95	14.30	
365	55.93	6.70	6.50	13.20	
366	55.96	5.60	6.70	12.30	
367	55.99	4.25	4.15	8.40	
368	56.02	5.00	6.40	11.40	
369	56.05	5.45	6.85	12.30	
370	56.08	6.70	6.80	13.50	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
371	56.11	7.45	4.95	12.40	
372	56.14	5.70	4.50	10.20	
373	56.17	5.40	4.90	10.30	
374	56.20	5.60	6.30	11.90	
375	56.23	6.45	5.95	12.40	
376	56.26	7.25	7.15	14.40	
377	56.29	6.75	6.65	13.40	
378	56.32	6.85	4.35	11.20	
379	56.35	3.95	4.25	8.20	
380	56.38	4.75	7.75	12.50	
381	56.41	5.30	6.00	11.30	
382	56.44	5.80	5.40	11.20	
383	56.47	5.30	5.70	11.00	
384	56.50	5.90	6.70	12.60	
385	56.53	5.70	5.60	11.30	
386	56.56	5.65	5.75	11.40	
387	56.59	6.20	5.30	11.50	
388	56.62	5.35	5.45	10.80	
389	56.65	4.90	7.60	12.50	
390	56.68	8.60	5.30	13.90	
391	56.71	6.65	6.15	12.80	
392	56.74	5.95	5.05	11.00	
393	56.77	5.05	6.25	11.30	
394	56.80	5.35	5.25	10.60	
395	56.83	5.00	7.50	12.50	
396	56.86	4.85	6.85	11.70	
397	56.89	6.50	5.80	12.30	
398	56.92	5.75	6.15	11.90	
399	56.95	5.55	6.15	11.70	
400	56.98	6.25	5.35	11.60	
401	57.01	6.00	5.70	11.70	
402	57.04	5.85	5.55	11.40	
403	57.07	5.75	5.95	11.70	
404	57.10	6.70	6.30	13.00	
405	57.13	8.30	6.40	14.70	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
406	57.16	6.75	6.25	13.00	
407	57.19	6.25	5.75	12.00	
408	57.22	7.35	6.75	14.10	
409	57.25	5.40	4.80	10.20	
410	57.28	4.20	4.10	8.30	
411	57.31	5.05	5.25	10.30	
412	57.34	6.50	5.90	12.40	
413	57.37	7.25	7.75	15.00	
414	57.40	8.95	5.95	14.90	
415	57.43	6.05	3.75	9.80	
416	57.46	6.00	6.90	12.90	
417	57.49	6.75	5.75	12.50	
418	57.52	7.00	5.50	12.50	
419	57.55	7.05	5.15	12.20	
420	57.58	7.35	5.45	12.80	
421	57.61	6.50	5.60	12.10	
422	57.64	6.15	6.35	12.50	
423	57.67	6.25	6.85	13.10	
424	57.7	6.55	5.95	12.50	
425	57.73	5.65	5.05	10.70	
426	57.76	4.30	5.20	9.50	
427	57.79	6.35	6.05	12.40	
428	57.82	4.80	5.30	10.10	
429	57.85	4.35	6.95	11.30	
430	57.88	5.20	6.50	11.70	
431	57.91	7.10	5.00	12.10	
432	57.94	6.40	5.70	12.10	
433	57.97	6.40	6.40	12.80	
434	58.00	5.10	4.90	10.00	
435	58.03	6.65	5.45	12.10	
436	58.06	8.70	6.30	15.00	
437	58.09	7.60	6.20	13.80	
438	58.12	5.55	7.15	12.70	
439	58.15	5.30	5.90	11.20	
440	58.18	5.25	7.15	12.40	

**Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)**

S.No.	Chainage(km)	Existing ROW (in M)			Remarks
		Left Side	Right Side	Total ROW	
441	58.21	5.70	7.20	12.90	
442	58.24	6.65	7.45	14.10	
443	58.27	6.90	6.10	13.00	
444	58.30	7.30	6.70	14.00	
445	58.33	7.35	5.75	13.10	
446	58.36	6.65	7.25	13.90	
447	58.39	7.75	5.25	13.00	
448	58.42	5.25	4.35	9.60	
449	58.45	7.35	4.35	11.70	
450	58.48	7.65	6.35	14.00	
451	58.51	6.55	6.35	12.90	
452	58.54	4.85	5.05	9.90	
453	58.57	5.85	5.95	11.80	
454	58.60	5.70	6.10	11.80	
455	58.63	5.70	6.90	12.60	
456	58.66	5.05	5.25	10.30	
457	58.69	5.75	5.35	11.10	
458	58.72	5.80	4.30	10.10	
459	58.75	4.80	4.70	9.50	
460	58.78	5.70	4.00	9.70	
461	58.81	3.70	4.30	8.00	
462	58.84	6.15	6.15	12.30	
463	58.87	7.90	6.10	14.00	
464	58.90	6.30	8.30	14.60	
465	58.93	5.65	6.05	11.70	
466	58.96	5.75	4.65	10.40	
467	58.99	6.50	6.70	13.20	
468	59.02	6.75	7.25	14.00	
469	59.05	8.05	5.55	13.60	
470	59.08	4.75	5.65	10.40	
471	59.11	7.45	6.45	13.90	
472	59.14	8.75	8.61	17.36	
473	59.17	9.55	8.85	18.40	
474	59.20	8.75	8.45	17.20	

Note :- Minimum encumbrances free RoW is 7.5m available all along the road.

### 3. Carriageway

The present carriageway of the Project Highway is of Single Lane/Intermediate lane carriageway flexible pavement having carriageway varying from 3.0m to 5.5m.

### 4. Major Bridges

The Site includes the following Major Bridges:

S. No.	Existing Chainage (km)	Type of Structure			No. of Spans with span length (m)	Width (m)
		Foundation	Sub-Structure	Super-Structure		
NIL						

### 5. Road over-bridges (ROB)/ Road under-bridges (RUB):

The Site includes the following ROB (road over railway line)/RUB (road under railway line):

S.No	Existing Chainage (km)	Type of Structure		No. of Spans with span length (m)	Width (m)	ROB/ RUB
		Foundation	Super Structure			
NIL						

### 6. Grade separators

The Site includes the following grade separators:

S. No	Existing Chainage (km)	Type of Structure		No. of Spans with span length (m)	Width (m)
		Foundation	Superstructure		
NIL					

### 7. Minor Bridges

The Site includes the following minor bridges

S. No.	Existing Chainage (km)	Type of Structure			No. of Spans with span length (c/c of exp gap)	Total Width (m)
		Foundation	Sub-Structure	Super-Structure		
1	58.322	Open	RC WALL	RC SOLID SLAB	1x7.1	8.0

### 8. Railway level crossings

The Site includes the following level crossing:

S. No.	Existing Chainage (km)	Remarks
NIL		

### 9. Underpasses (Vehicular, Non Vehicular)

The Site includes the following underpasses:

S. No.	Existing Chainage (Km)	Type of Structure	No. of Spans with span length (m)	Width (m)
NIL				

### 10. Culverts

The Site has the following culverts:

S. No	Existing Chainage (Km)	Type of culvert	Span/ Opening with Span length (m)
1	45.448	Slab	1x1
2	46.240	Slab	1x0.8
3	47.392	Slab	1x0.9
4	48.777	Slab	1x4.4
5	49.200	Slab	1x3.0
6	49.410	HP	1x0.9
7	49.570	Slab	1x0.8
8	49.657	HP	1x0.9
9	49.775	Slab	1x0.7
10	49.900	Slab	1x0.9
11	50.064	HP	1x0.9
12	50.153	HP	1x0.9
13	50.272	HP	1x0.9
14	50.342	HP	1x0.9
15	50.525	HP	1x0.9
16	50.907	HP	1x0.9
17	51.181	HP	3x.9
18	51.240	HP	1x.9
19	51.500	HP	1x.9

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

S. No	Existing Chainage (Km)	Type of culvert	Span/ Opening with Span length (m)
20	51.600	HP	1x.9
21	51.906	Slab	1x4.4
22	52.152	HP	1x.9
23	52.256	Slab	1x4.3
24	52.700	Slab	1x4.2
25	52.950	HP	1x0.9
26	53.162	HP	1x0.9
27	53.452	HP	1x0.9
28	53.664	HP	1x0.9
29	53.794	HP	1x0.9
30	54.146	HP	1x0.9
31	54.340	HP	1x0.9
32	54.430	Slab	1x4.6
33	54.650	HP	1x0.9
34	54.812	HP	1x0.9
35	55.102	Slab	1x4.6
36	55.324	HP	1x0.9
37	55.482	HP	1x0.9
38	55.594	HP	1x0.9
39	55.904	Slab	1x2.8
40	55.250	Slab	1x2.8
41	56.456	HP	2x.9
42	56.721	HP	1x0.9
43	56.803	HP	1x0.9
44	57.275	Slab	1x4.7
45	57.525	HP	1x0.9
46	57.670	HP	3x0.9
47	57.728	HP	3x0.9
48	57.823	HP	1x0.9
49	57.908	HP	1x0.9

S. No	Existing Chainage (Km)	Type of culvert	Span/ Opening with Span length (m)
50	58.145	HP	1x0.9
51	58.725	Slab	1x3.7
52	59.050	Slab	1x4.4

### 11. Bus bays/Bus Shelters

The details of bus shelters on the Site are as follows:

S.No.	Existing Chainage	Side
1	53.545	Right
2	54.155	Right
3	55.013	Right

### 12. Truck Lay byes

The details of truck lay byes are as follows:

S. No.	Existing Chainage (Km)	Length (m)	Left Hand Side	Right Hand Side
NIL				

### 13. Road side drains

The details of the roadside drains are as follows:

S.No.	Existing Chainage (km)		Side
	From	To	
1	47.983	48.168	Right
2	51.284	51.375	Left
3	51.400	51.477	Left
4	51.639	51.700	Left
5	51.700	51.729	Both Side
6	52.200	52.452	Left
7	52.475	52.775	Left
8	53.293	53.300	Left
9	53.400	53.468	Left
10	54.583	54.600	Left
11	55.955	56.172	Both Side
12	56.600	56.652	Left

S.No.	Existing Chainage (km)		Side
	From	To	
13	56.673	56.800	Left
14	56.875	57.200	Left
15	57.300	57.822	Left
16	57.863	57.900	Left
17	57.947	58.092	Left
18	58.092	58.120	Both Side
19	58.175	58.241	Left
20	58.241	58.294	Both Side
21	58.294	58.327	Left

#### 14. Major junctions

The details of major junctions are as follows:

S.No	Existing Chainage (km)	At Grade	Grade Separated	Category of Cross Road+			
				NH	SH	MDR	Others
NIL							

+ NH= National Highway, SH= State Highway, MDR= Major District Road.

#### 15. Minor junctions

The details of the minor junctions are as follows:

S. No	Existing Chainage (km)	Village Name	Side	Type of Junction
1	53.309	To Miletalak Village	LHS	Y-type
2	58.800	To Ranghali	LHS	Y-type

#### 16. Bypasses

The details of the existing road sections proposed to be bypasses are as follows:

S. No	Name of bypass (Town)	Existing Chainage (Km)		Length (Km)
		From	To	
NIL				

#### 17. Other Structures : Following are the details of existing causeways:

S. No	Existing Chainage (km)	Structure Type	Openings / Spans X Length	Width (m)
NIL				

**Annex - II**  
(Schedule-A)

**Dates for providing Right of Way**

The dates on which the Authority shall provide Right of Way to the Contractor on different stretches of the Site are stated below:

S. No.	Design Chainage (Km)		Design Length (Km)	Width (In Meter)	Dates of Providing ROW
	From	To			
1	2	3	4	5	6
	<b>Part Right of Way</b>				
	Width of Land as per Clause 2 of Annexure-I of Schedule A				On Appointed Date

**Annex-III**  
*(Schedule-A)*

**Alignment Plans**

The existing alignment of the Project Highway shall be modified in the following sections as per the alignment plan indicated below:

(i) The alignment of the Project Highway is enclosed in alignment plan. Finished road level indicated in the alignment plan shall be followed by the contractor as minimum FRL. In any case, the finished road level of the project highway shall not be less than those indicated in the alignment plan. The contractor shall, however, improve/upgrade the Road profile as indicated in Annex-III based on site/design requirement.

(ii) Traffic Signage plan of the Project Highway showing numbers & location of traffic signs is enclosed. The contractor shall, however, improve/upgrade upon the traffic signage plan as indicated in Annex-III based on site/design requirement as per IRC: SP: 99 & IRC: 67

**Annex - IV**

*(Schedule-A)*

**Environment Clearances**

Not Applicable for this section.

## **SCHEDULE - B**

*(See Clause 2.1)*

### **Development of the Project Highway**

**1. Development of the Project Highway**

Development of the Project Highway shall include design and construction of the Project Highway as described in this Schedule-B and in Schedule-C.

**2. Rehabilitation and Upgradation**

Rehabilitation and Upgradation shall include Intermediate lane with Hard shoulder of the Project Highway as described in Annex-I of this Schedule-B and in Schedule-C.

**3. Specifications and Standards**

The Project Highway shall be designed and constructed in conformity with the Specifications and Standards specified in Annex-I of Schedule-D.

## Annex - I

(Schedule-B)

### Description of Two-Laning

#### Project Description:-

Rehabilitation and Upgradation of NH-4 (Old NH-223) popularly known as Andaman Trunk Road (ATR) has been entrusted to NHIDCL for the entire stretch of 330.7 Km distributed in South Andaman and North & Middle Andaman. In south Andaman the stretch from Km 12 (Beodnabad) to Km 45 (Ferrargunj) (Excluding Km 21 to Km 28) is being upgraded to two lanes. The rehabilitation and Upgradation of the most important stretch i.e from Km 45 (Ferrargunj) to km 59.20 (Jirkatang) is proposed by upgrading to 5.5 m and 0.5m hard shoulder on both sides. The road is to be constructed on the existing alignment only. There is no realignment

#### 1. WIDENING OF THE EXISTING HIGHWAY

1.1 The Project Highway shall follow the existing alignment unless otherwise specified by the Authority and shown in the alignment plans specified in Annex III of Schedule-A. Geometric deficiencies, if any, in the existing horizontal and vertical profiles shall be corrected as per the prescribed standards for plain/rolling terrain to the extent land is available.

#### 1.2 WIDTH OF CARRIAGEWAY

1.2.1 Intermediate-Lane with hard shoulders in rural section shall be undertaken. The carriageway shall be 5.5m wide in rural section conformation with the typical cross sections drawings in the Manual.

Provided that in the built-up areas the width of the carriageway shall be as specified in the following table:

S.No.	Built up Stretch (Township)	Design Chainage (Km)		Length (km)	Typical Cross Section Proposed
		From	To		
NIL					

1.2.2 Except as otherwise provided in this Agreement, the width of the paved carriageway and cross-sectional features shall conform to paragraph 1.2.1 above

#### 2. GEOMETRIC DESIGN AND GENERAL FEATURES

##### 2.1 General

Geometric design and general features of the Project Highway shall be in accordance with section 2 of the manual.

## 2.2 Design Speed

The design speed shall in accordance with section 2 of the manual.

## 2.3 Improvement of the existing road geometry

In the following sections, where improvement of the existing road geometrics to the prescribed standards is not possible within existing ROW, the existing road geometrics shall be improved to the extent possible within the given right of way and proper road signs and safety measures shall be provided:

### Deficient Curves:-

S.No	Design Chainage (km)	Radius (m)
1	46.401	50
2	47.895	40
3	48.294	60
4	48.401	60
5	48.918	50
6	49.026	35
7	49.159	50
8	49.270	50
9	49.347	30
10	49.456	50
11	49.543	30
12	49.675	50
13	49.933	50
14	50.013	50
15	50.071	50
16	50.151	50
17	50.243	25
18	50.317	30
19	50.726	70
20	50.857	30
21	50.932	30
22	51.041	25
23	51.281	50

S.No	Design Chainage (km)	Radius (m)
24	51.921	70
25	52.057	50
26	52.308	50
27	52.373	50
28	52.446	70
29	53.092	40
30	53.496	60
31	56.170	22
32	56.242	30

### Bypasses

S. No	DesignChainage (Km)		Length (Km)	Name of village	Remarks
	From	To			
NIL					

## 2.4 Right of Way

The Site of the Project Highway comprises the land as described in Annexure-I of Schedule-A.

## 2.5 Type of Shoulders

Paved shoulder in built up area and Hard shoulder with CTSB (cementations treated sub base) in other areas for impervious quality.

a) In built up section, footpath over drain shall be provided in the following stretches:

S.No.	Built up Stretch (Township)	Design Chainage (Km)		Typical Cross Section Proposed
		From	To	
NIL				

b) Design and specification of paved shoulder and granular material shall confirm to the requirements specified in paragraph 5.9.9 and 5.9.10 of the Manual.

c) In built up area full road width to be paved whereas in other area hard shoulder of 0.5m either side with CTSB is to be done.

## 2.6 Lateral and vertical clearances at underpasses

2.6.1 Lateral and vertical clearances at underpasses and provision of guardrails/crash barriers shall be as per paragraph 2.11 of the Manual.

2.6.2 Lateral clearance: The width of the opening at the underpasses shall be as follows:

S. No.	Design Chainage (Km)	Span/opening (m)	Remarks
NIL			

## 2.7 Lateral and vertical clearances at overpasses

2.7.1 Lateral and vertical clearances at overpasses and provision of guardrails/crash barriers shall be as per paragraph 2.12 of the Manual.

2.7.2 Lateral clearance: The width of the opening at the overpasses shall be as follows:

S. No.	Design Chainage (Km)	Span/opening (m)	Remarks
NIL			

## 2.8 Service roads

Service roads/Slip Roads shall be constructed at the locations and for the lengths indicated below:

S. No	Design Chainage (Km)	RHS/LHS	Length of the Service Road (m)
NIL			

## 2.9 Grade separated structures

2.9.1 Grade separated structures shall be provided as per paragraph 2.14 of the Manual. The requisite particulars are given below:

S. No.	Design Chainage (Km)	Length (m)	Number and length of spans	Approach gradient	Remarks, if any
NIL					

2.9.2 In the case of grade separated structures, the type of structure and the level of the Project Highway and the cross roads shall be as follows:

S. No.	Design Chainage (Km)	Type of structure Length (m)	Cross road at		
			Existing level	Raised Level	Lowered Level
NIL					

## 2.10 Cattle and pedestrian under pass / over pass

Cattle and pedestrian underpass shall be constructed as follows:

S. No.	Design Chainage (Km)	Type of Crossing
NIL		

## 2.11 Typical cross-sections of the Project Highway

Indicative typical cross sections along with different types of cross-sections required to be developed in different segments of the project highway are indicated in Appendix B-I. Cross Section schedule for the project highway is as follows:

S.No	Design Chainage		Widening Proposal	Length (m)	TCS Proposed
	From	To			
1	44+213	44+300	Recons.-Rural	87	TCS I
2	44+300	44+385	Recons.-Rural	85	TCS II*
3	44+385	45+050	Recons.-Rural	665	TCS I
4	45+050	45+175	Recons.-Rural	125	TCS II*
5	45+175	45+625	Recons.-Rural	450	TCS I
6	45+625	45+800	Recons.-Rural	175	TCS II*
7	45+800	45+875	Recons.-Cutting	75	TCS III**
8	45+875	46+350	Recons.-Rural	475	TCS II*
9	46+350	46+625	Recons.-Filling	275	TCS IV***
10	46+625	46+775	Recons.-Rural	150	TCS I
11	46+775	46+910	Recons.-Filling	135	TCS IV***
12	46+910	47+100	Recons.-Rural	190	TCS I
13	47+100	47+175	Recons.-Rural	75	TCS II*
14	47+175	47+275	Recons.-Rural	100	TCS I
15	47+275	47+425	Recons.-Rural	150	TCS II*
16	47+425	47+925	Recons.-Rural	500	TCS I
17	47+925	48+180	Recons.-Rural	255	TCS II*
18	48+180	48+265	Recons.-Cutting	85	TCS III**
19	48+265	48+375	Recons.-Rural	110	TCS I
20	48+375	48+575	Recons.-Rural	200	TCS II*
21	48+575	48+630	Recons.-Cutting	55	TCS III**
22	48+630	49+000	Recons.-Rural	370	TCS II*
23	49+000	49+060	Recons.-Cutting	60	TCS III**
24	49+060	49+200	Recons.-Rural	140	TCS I
25	49+200	49+325	Recons.-Rural	125	TCS II*
26	49+325	49+450	Recons.-Rural	125	TCS I
27	49+450	49+525	Recons.-Rural	75	TCS II*

**Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)**

S.No	Design Chainage		Widening Proposal	Length (m)	TCS Proposed
	From	To			
28	49+525	49+725	Recons.-Rural	200	TCS I
29	49+725	49+875	Recons.-Rural	150	TCS II*
30	49+875	50+000	Recons.-Rural	125	TCS I
31	50+000	50+125	Recons.-Rural	125	TCS II*
32	50+125	50+225	Recons.-Rural	100	TCS I
33	50+225	50+330	Recons.-Cutting	105	TCS III**
34	50+330	50+640	Recons.-Rural	310	TCS I
35	50+640	50+720	Recons.-Cutting	80	TCS III**
36	50+720	51+200	Recons.-Rural	480	TCS I
37	51+200	52+650	Recons.-Rural	1450	TCS II*
38	52+650	52+750	Recons.-Cutting	100	TCS III**
39	52+750	53+625	Recons.-Rural	875	TCS II*
40	53+625	53+750	Recons.-Cutting	125	TCS III**
41	53+750	54+875	Recons.-Rural	1125	TCS II*
42	54+875	54+960	Recons.-Cutting	85	TCS III**
43	54+960	55+200	Recons.-Rural	240	TCS II*
44	55+200	55+350	Recons.-Cutting	150	TCS III**
45	55+350	56+050	Recons.-Rural	700	TCS II*
46	56+050	56+250	Recons.-Cutting	200	TCS III**
47	56+250	56+325	Recons.-Rural	75	TCS II*
48	56+325	56+425	Recons.-Filling	100	TCS IV***
49	56+425	56+575	Recons.-Cutting	150	TCS III**
50	56+575	57+150	Recons.-Rural	575	TCS II*
51	57+150	57+230	Recons.-Cutting	80	TCS III**
52	57+230	57+800	Recons.-Rural	570	TCS II*
53	57+800	57+880	Recons.-Filling	80	TCS IV***
54	57+880	58+307	Recons.-Rural	427	TCS II*

**[Typical Cross Sections are appended separately]**

- \* Retaining wall and Breast wall shown in TCS II drawing is typical. Location of these components should be applied as per site condition.
- \*\* TCS III is proposed for cutting in rural section and drawing shown for the same is typical. Wherever required the breast wall must be provided, if there is cutting in hill.
- \*\*\* TCS IV is proposed for filling in rural section and drawing shown for the same is typical. Wherever required the retaining wall must be provided, to confine the toe within ROW.

### 3. INTERSECTIONS AND GRADE SEPARATORS

All intersections and grade separators shall be as per section 3 of the Manual. Existing intersections which are deficient shall be improved to the prescribed standards within the available road width only. Junction road to be developed upto 50 m length only.

Properly designed intersections shall be provided at the locations and of the types and features given in the table below:

#### a) At-grade intersections (Major Junctions)

S. No.	Design Chainage (Km)	Type of Intersection	Other features	Remarks
NIL				

#### b) At-grade intersections (Minor Junctions)

S.No	Design Chainage (km)	Type of Intersection
1	52+460	Y-type
2	57+927	Y-type

#### c) Grade separated intersection without ramps

S. No.	Design Chainage (Km)	Salient features	Minimum length of viaduct to be provided	Road to be carried over/under the structures
NIL				

### 4. ROAD EMBANKMENT AND CUT SECTION

- 4.1 Widening and improvement of the existing road embankment/cuttings and construction of new road embankment/ cuttings shall conform to the Specifications and Standards given in section 4 of the Manual and the specified cross sectional details. Deficiencies in the plan and profile of the existing road shall be corrected.
- 4.2 Raising of the existing road

The existing road shall be raised at the required locations as per proposed plan and profile including the following sections:

S. No	Design Chainage (Km)		Length (Km)	Extent of raising (Top of finished road level)
	From	To		
NIL				

## 5. PAVEMENT DESIGN

5.1 Pavement design shall be carried out in accordance with Section 5 of the Manual.

### 5.2 Type of pavement

Flexible Pavement has been designed by the DPR Consultant as per Clause 10.4 (Treated RAP) of IRC: 37-2012 along with soil stabilization for 3 CBR and 10 msa.

### 5.3 Design Requirements

Design requirement for the flexible pavement shall be in accordance with section 5 of the IRC:SP-73-2015 and IRC:37-2012. Treated RAP and CT Subbase has been considered by the DPR Consultant as per the provisions of IRC: 37-2012.

#### 5.3.1 Design Period and strategy

Flexible pavement for new pavement or for widening and strengthening of the existing pavement shall be designed for a minimum design period of **15 years**. Stage construction shall not be permitted.

#### 5.3.2 Design Traffic

Not with standing anything to the contrary contained in this Agreement or the Manual, the Contractor shall design the pavement for design traffic as given below.

S. No.	Design Chainage (Km)		Minimum Design MSA for 15 yrs
	From	To	
1	44+213	58+307	10

## 5.4 Reconstruction of stretches

Reconstruction of stretches for matching the proposed plan & profile shall be taken up as per actual requirements.

S. No	Design Chainage (Km)		Remarks
	From	To	
1	44+213	58+307	Reconstruction with Treated RAP & CT subbase

## 5.5 Maintenance before Construction:-

A Provision of 20 mm of premix carpet with seal coat for 10% of the existing pavement to maintain the trafficability of the road before construction. The contractor shall maintain the road in the best possible manner to provide smooth trafficability. The Premix carpet to be used only on those places which shall be taken up for the construction after a period of minimum 2 months. 6300 Sqm area is being repaired by NHIDCL in the month of Oct/Nov 2018 to maintain the road as pothole free. Therefore this requirement is provisional only and not mandatory for execution. The contractor has to maintain the road in accordance with Clause 10.4.1 of the Draft Contract Agreement as pot hole free road only during construction.

Note: - Method for flexible pavement designing has been extracted from Plate 17 with 3% CBR value and 10 Msa of Fig 10.4 of IRC: 37 – 2012 with BC of 40mm. However the designs are indicative only and the contractor can submit the design as per the requirement. In all cases 40mm BC is mandatory requirement.

## 6. ROADSIDE DRAINAGE

Drainage system including surface and subsurface drains for the Project Highway shall be provided as per Section 6 of the Manual.

Unlined drain has to be provided in complete stretch except at locations of breast wall and retaining walls. Minimum length of 14467m to be constructed.

Lined Drain of Random Rubble Masonary has been provided in hilly sections at the locations of Breast wall. Minimum length of 1965m to be constructed.

## 7. DESIGN OF STRUCTURES

### 7.1 General

7.1.1 All bridges, culverts and structures shall be designed and constructed in accordance with section 7 of the manual and shall conform to the cross-sectional features and other details specified therein.

7.1.2 Width of the carriageway of new structures of more than 60m length shall be as follows, if the carriageway width is different from 7.5m in the table below.

S.No	Design Chainage (Km)	Width of Carriageway (m) and cross sectional features
NIL		

7.1.3 The following structures shall be provided with footpaths:

S. No.	Design Chainage (Km)	Remarks
NIL		

7.1.4 All bridges shall be high-level bridges. No

7.1.5 The following structures shall be designed to carry utility services specified in table below.

S. No.	Design Chainage (Km)	Utility service to be carried	Remarks
NIL			

7.1.6 Cross-section of the new culverts and bridges at deck level for the Project Highway shall conform to the typical cross-sections for the Project Highway.

## 7.2 Culverts

7.2.1 The Culverts overall width shall be equal to 7.5m.

### 7.2.2 *Reconstruction of existing culverts:*

The existing culverts at the following locations shall be re-constructed as new culverts.

S. No	Design Chainage (Km)	Type of culvert proposed	Span/ Opening with Span length (m)*	Remarks
1	48+505	Box	1x2.0	
2	48+753	Box	1x2.0	
3	49+208	Box	1x2.0	
4	49+299	Box	1x2.0	
5	49+414	Box	1x2.0	
6	49+483	Box	1x2.0	
7	49+671	Box	1x2.0	
8	50+050	Box	1x2.0	
9	50+223	Box	1x2.5	
10	50+376	Box	1x2.0	
11	50+636	Box	1x2.0	
12	50+731	Box	1x2.0	
13	51+283	Box	1x2.0	
14	51+388	Box	1x4.5	
15	52+076	Box	1x2.0	
16	52+300	Box	1x2.0	
17	52+591	Box	1x2.0	
18	52+800	Box	1x2.0	
19	52+931	Box	1x2.0	
20	53+284	Box	1x2.0	

S. No	Design Chainage (Km)	Type of culvert proposed	Span/ Opening with Span length (m)*	Remarks
21	53+477	Box	1x2.0	
22	53+788	Box	1x2.0	
23	53+947	Box	1x2.0	
24	54+446	Box	1x2.0	
25	54+607	Box	1x2.0	
26	54+717	Box	1x2.0	
27	55+591	Box	1x2.0	
28	55+850	Box	1x2.0	
29	55+938	Box	1x2.0	
30	56+655	Box	1x2.0	
31	56+796	Box	1x2.5	
32	56+858	Box	1x2.5	
33	56+950	Box	1x2.0	
34	57+034	Box	1x2.0	
35	57+266	Box	1x2.0	
36	57+846	Box	1x4.0	
37	58+159	Box	1x4.5	

\*Note- height of opening shall be kept according to adjoining TCS.

### 7.2.3 Widening of Existing Culverts

All existing culverts, which are not to be reconstructed, shall be widened up to the roadway width of the Project Highway & as per the typical cross section given in the Manual and the existing width portion of culverts shall be repaired as per site requirements.

S. No	Design Chainage (Km)	Structure Type	Openings / Spans x Length	Width of existing culvert (m)	Remark
NIL					

7.2.4 Additional new culverts (given in table below) shall be constructed for width equal to the roadway width of the Project Highway & as per typical cross-section given in the manual:

S. No.	Design Chainage (Km)	Proposed type	No. of Spans X span length (m)
NIL			

7.2.5 Repairs/replacements of railing/parapets **as required**, flooring and protection works of the existing culverts shall be undertaken as follows:

S. No.	Design Chainage (Km)	Type of repair required
1	44+659	Cleaning, Shotcreting, Painting, Pointing, Repair of railing/parapets
2	45+421	
3	46+567	
4	47+937	
5	48+300	
6	48+665	
7	48+871	
8	48+996	
9	51+037	
10	51+830	
11	53+568	
12	54+226	
13	55+028	
14	55+380	
15	56+403	

7.2.6 Floor protection works shall be as specified in the relevant IRC Codes and Specifications

7.2.7 Provision in Schedule H has been kept for repair of all existing culverts other than reconstruction shall be done including cleaning, maintenance, pointing, painting etc in all respect.

### 7.3 Bridges

7.3.1 Existing bridges to be re-constructed/widened:

(i) The Existing bridges at the following locations shall be reconstructed:

S. No	Bridge Location (Design Chainage, in Km)	Salient Features of Existing Bridge		Features of Proposed Bridge	
		No. of Spans with Span Length (c/c of exp. Gap)	Total Width (m)	Proposed Length (m)	Total proposed Width
NIL					

NOTE: GAD is given in CD

(ii) The following narrow bridges shall be widened:

S.No.	Design Chainage (Km)	Width (m)	Extent* of Widening	Span Arrangement (m)	Type of Structure			Cross Section at Deck Level for widening
					Foundation	Sub-Structure	Super-Structure	
NIL								

### 7.3.2 Additional new bridges

New bridges at the following locations on the project highway shall be constructed. GADs for the new bridges are attached in the drawings folder.

S. No.	Bridge Location (Design Chainage, in Km)	Total Length (m)	Remarks
NIL			

7.3.3 The railings of existing bridges shall be replaced by crash barriers at the following locations:

S. No.	Design Chainage (Km)	Total length (m)	Remarks
NIL			

7.3.4 Repairs/replacements of railing/parapets of the existing bridges shall be undertaken as follows

S. No.	Design Chainage (Km)	Existing span arrangement (m)	Remarks
NIL			

### 7.3.5 Drainage system for bridge deck

An effective drainage system for bridge decks shall be provided as specified in paragraph 7.21 of the Manual

### 7.3.6 Structures in marine environment

The Project Alignment does not lie in Marine Alignment.

7.3.7 Provision in Schedule H has been kept for repair of all existing minor bridges other than reconstruction shall be done including cleaning, maintenance, pointing, painting etc in all respect.

#### 7.4 Rail-road bridges

7.4.1 Design, construction and detailing of ROB/RUB shall be as specified in the Manual. The Width of proposed ROB shall be as specified in Schedule D.

#### 7.4.2 Road over-bridges

Road under-bridges (road under railway line) shall be provided at the following level crossings, as per GAD drawings attached:

S. No	Proposed Structure	Existing Chainage	Design Chainage	Name of Crossing	Proposed structural configuration	Proposed Super Structure	Proposed span arrangement (m)	Total Width of Structure
NIL								

#### 7.4.3 Road under-bridges

Road under-bridges (road under railway line) shall be provided at the following level crossings, as per GAD drawings attached:

S. No	Design Chainage (Km)	Number and length of span (m)
NIL		

#### 7.5 Grade separated structures

The grade separated structures shall be provided at the locations and of the type and length specified in paragraphs 2.9 and 3 of this Annex-I.

#### 7.6 Repairs and strengthening of bridges and structures

The existing bridges and structures to be repaired / strengthened **as required**, and the nature and extent of repairs /strengthening required are given below:

##### A. Bridges

S. No.	Design Chainage (Km)	Nature and extent of repairs /strengthening to be carried out
1	57+448	Minor repair works (Cleaning, Shotcreting, Painting, Pointing, Replacement of railing with Crash Barrier, repair of Expansion joints & bearings, replacement of wearing coat, etc.)

##### B. ROB / RUB

S. No.	Design Chainage (Km)	Nature and extent of repairs /strengthening to be carried out
NIL		

### C. Overpasses/Underpasses and other structures

S. No.	Design Chainage (Km)	Nature and extent of repairs /strengthening to be carried out
NIL		

#### 7.7 List of Major Bridges and Structures

The following is the list of the Major Bridges and Structures:

S. No.	Type of Structure	Design Chainage (Km)	Remark
NIL			

- Note:** - 1. The location and vent size of all the culverts proposed for irrigation purposes shall be decided in consultation with irrigation authority/ independent engineer.
2. Width of culvert shall be reconciled as per cross section at that location
3. Cross road culvert to be provided at the location of Major Junction/ Minor Junctions or utility purposes etc. shall be decided with independent Engineer shall not be treated as change of scope.

#### 8. TRAFFIC CONTROL DEVICES AND ROAD SAFETY WORK.

- 8.1** Traffic control devices and road safety works including road markings shall be provided in accordance with Section 9 of the IRC:SP:73-2015.
- 8.2** Specifications of the reflective sheeting shall be as per the Manual of Specifications (IRC: SP: 73-2015).

#### 9. ROAD SIDE FURNITURE

- 9.1** Road side furniture shall be provided in accordance with the provisions of Section 11 of the IRC: SP: 73-2015.
- a) Road boundary stones for the entire project highway.
  - b) Pedestrian guard rails: At each bus stop location.
  - c) Delineators: For the entire project highway at the locations as suggested in schedule D.
- 9.2 Overhead traffic signs: location and size**
- a) Full width overhead signs: 2 Nos.
  - b) Cantilever overhead signs: Nil
  - c) Overhead Traffic Signs (locations & Size) shall conform to the Manual of Specifications (IRC:SP:73-2015).

#### 10. COMPULSORY AFFORESTATION

Not Required.

## 11. HAZARDOUS LOCATIONS

The road side safety/Crash barriers shall be provided at following locations for minimum length as per the Manual of Specifications (IRC:SP:73-2015). However, the actual length shall be identified as per requirement of clause 9.4 of IRC:SP:73-2015 in consultation with Authority Engineer. Any increase or decrease in length as specified shall not be treated as change of scope.

S.No	Design Chainage	Unit	Length
1	52+057	Rm	300
2	52+308	Rm	300
3	52+373	Rm	300
4	53+092	Rm	300
5	56+170	Rm	300
6	56+242	Rm	300
Total			<b>1800</b>

## 12. SPECIAL REQUIREMENTS FOR HILL ROAD

In accordance with the section 13 of the manual (IRC: SP 73:2015 & IRC: SP 48:1998) and recommended practices for the treatment of embankment and road side slopes erosion control (First Revision), IRC: 56-2011 and relevant IRC.

### 12.1 Slope Protection

As the project involves cutting of existing hill slope, it is imperative that slope are stabilized for ensuring longevity of the slope and the road. Slope stability, erosion control and landslide correction shall be accomplished in accordance with IRC: SP: 48-1998, IRC SP: 116-2018 and IRC SP: 23-2014. Reference may be drawn from IRC: 56-2011.

(i) The Minimum Quantity of Protection work may be taken as below:-

Type of Protection Work	Unit	Quantity
Breast wall	Rm	200
Retaining wall	Rm	1500

Breast Wall have been proposed along the roadway edge on the hilly side of the section of project road where cutting is required or cutting is more than available ROW. In hilly sections, breast Wall of PCC M-15 shall be provided.

Breast wall and Retaining wall shall be provided as specified in table below & in accordance with the Manual of Specifications and Standards as referred in Schedule-D.

**Breast Wall locations**

S.No	Design Chainage		Side	Length (m)
	From	To		
1	51+100	51+150	Left	50
2	51+320	51+370	Left	50
3	56+150	56+250	Left	100
<b>Total length (m)</b>				<b>200</b>

Retaining wall shall be proposed to be installed in sections of the project road having filling embankment height > 3m or toe of the filling section is beyond available ROW to confine it within ROW. Retaining wall of Random Rubble Masonry shall be provided.

**Retaining Wall locations**

S.No	Design Chainage		Side	Length (m)
	From	To		
1	46.350	46.400	left	50
2	46.400	46.500	Right	100
3	46.800	47.000	Left	200
4	48.200	48.400	Right	200
5	48.900	49.350	Right	450
6	51.000	51.100	Right	100
7	53.450	53.550	Right	100
8	56.400	56.550	Right	150
9	57.400	57.550	Right	150
<b>Total length (m)</b>				<b>1500</b>

**(ii) Passing Places**

18 no's of passing places has been provided in staggered manner along the alignment so as to avoid any hindrance in the movement of vehicles as in this package only intermediate lane has been proposed. The locations of these passing places shall be finalized by the Contractor in consultation with Authority's Engineer.

S.No	Design Chainage	Side
1	44+470	Left
2	44+540	Right
3	45+250	Left

4	45+320	Right
5	46+580	Left
6	46+650	Right
7	48+900	Right
8	49+100	Left
9	52+300	Right
10	52+410	Left
11	53+490	Left
12	53+550	Right
13	54+970	Left
14	55+100	Right
15	56+270	Left
16	56+360	Right
17	58+040	Right
18	58+135	Left

**(iii) Metal Beam Crash Barrier**

Metal beam has been provided at curve locations where radius is less than or equal to 50m. At every location it has been provided in the length of 150m on both sides. Locations of metal beam crash barrier are as tabulated below:

S.No	Design Chainage
1	52+057
2	52+308
3	52+373
4	53+092
5	56+170
6	56+242

**Note** – The Contractor shall be responsible for accurate assessment of the actual requirement as per site situation & prepared design for slope protection & stabilization as per the specification & standard stipulated in schedule ‘D’ and submit the same to the AE for review through the proof consultant and implement it accordance thereafter.

**Any Increase in quantity over and above the tentative quantity as mentioned in the above table or through change in specification will not be considered as change of scope.** Therefore contractor shall make thorough investigation at site and assess the requirement of slope protection and slide prone zone and other safety features at his own

before submission of bid.

**12.2 ROAD LAND BOUNDARY (Clause 12.2 IRC SP: 73: 2015)**

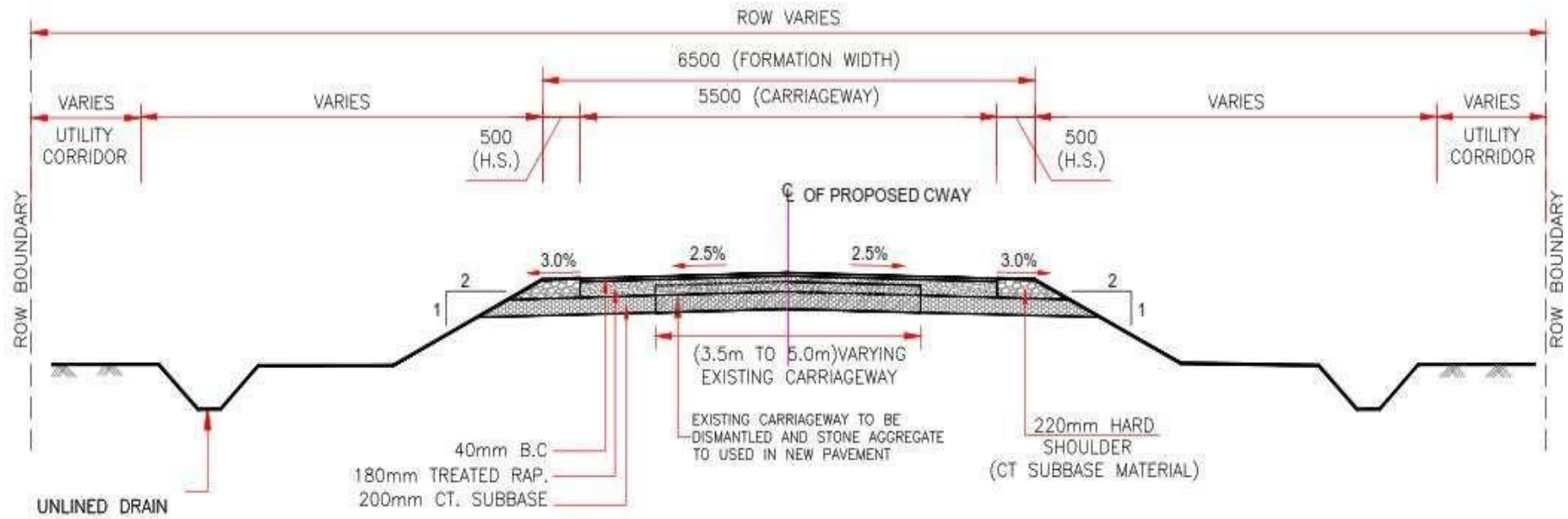
Road land (ROW) boundary shall be demarcated by putting RCC boundary pillars of size 60cm x 15cm x 15cm embedded in concrete (as per IRC:25) along the Project highways at 200 m interval on both side. All the components used in delineating road land boundary shall be aesthetically pleasing, sturdy and vandal proof. The road land boundary shall be demarcated in consultation with NHIDCL.

**12.3 Disposal of Debris – As per Manual**

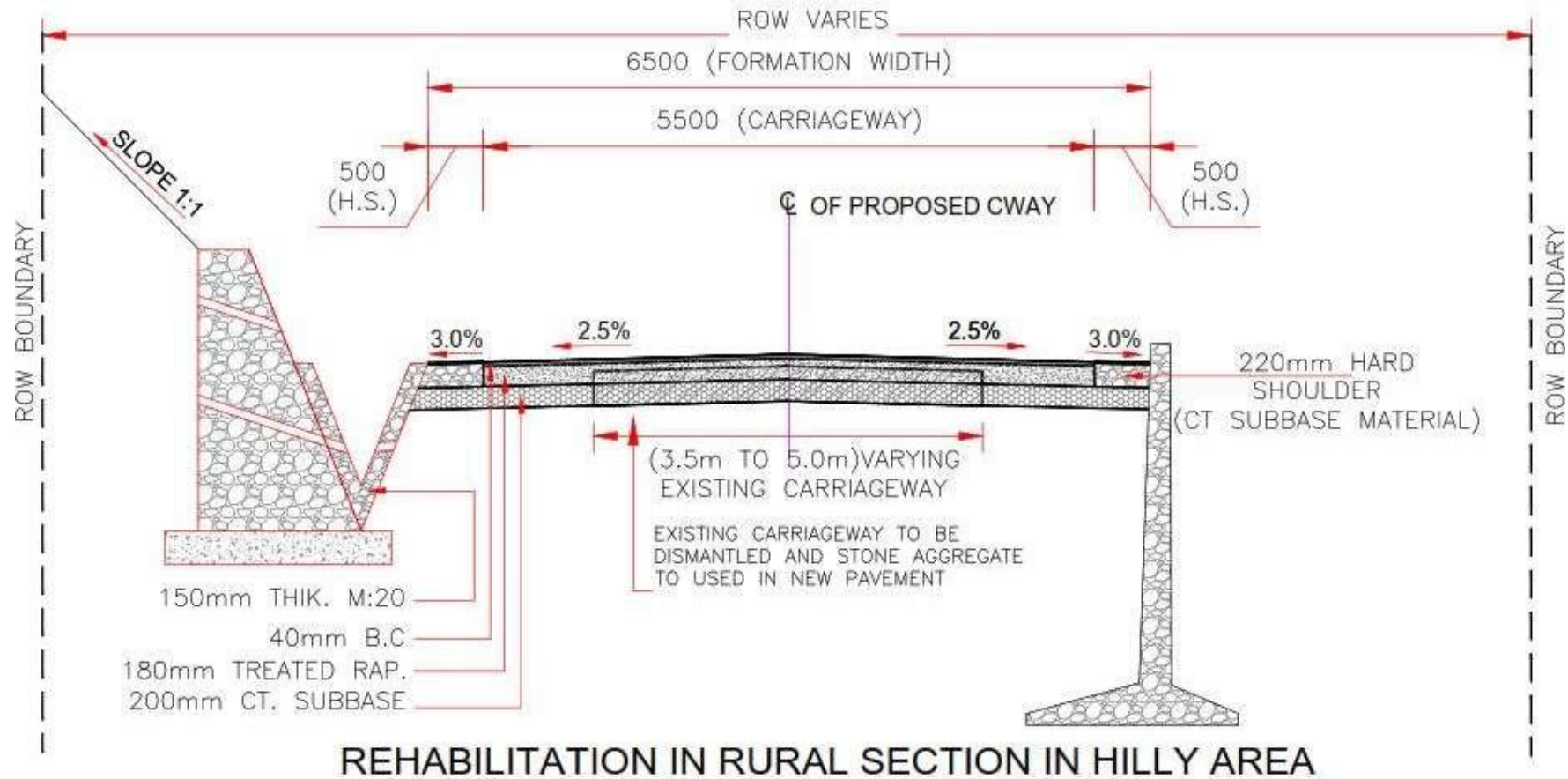
**13. CHANGE OF SCOPE**

The length of Structures and bridges specified hereinabove shall be treated as an approximate assessment. The Contractor in accordance with the Specifications and Standards shall determine the actual lengths as required on the basis of detailed investigations. Any variations in the lengths specified in this Schedule-B shall not constitute a Change of Scope, save and except any variations in the length arising out of a Change of Scope expressly undertaken in accordance with the provisions of Article 13.

### Appendix-B-I



**TCS-I**

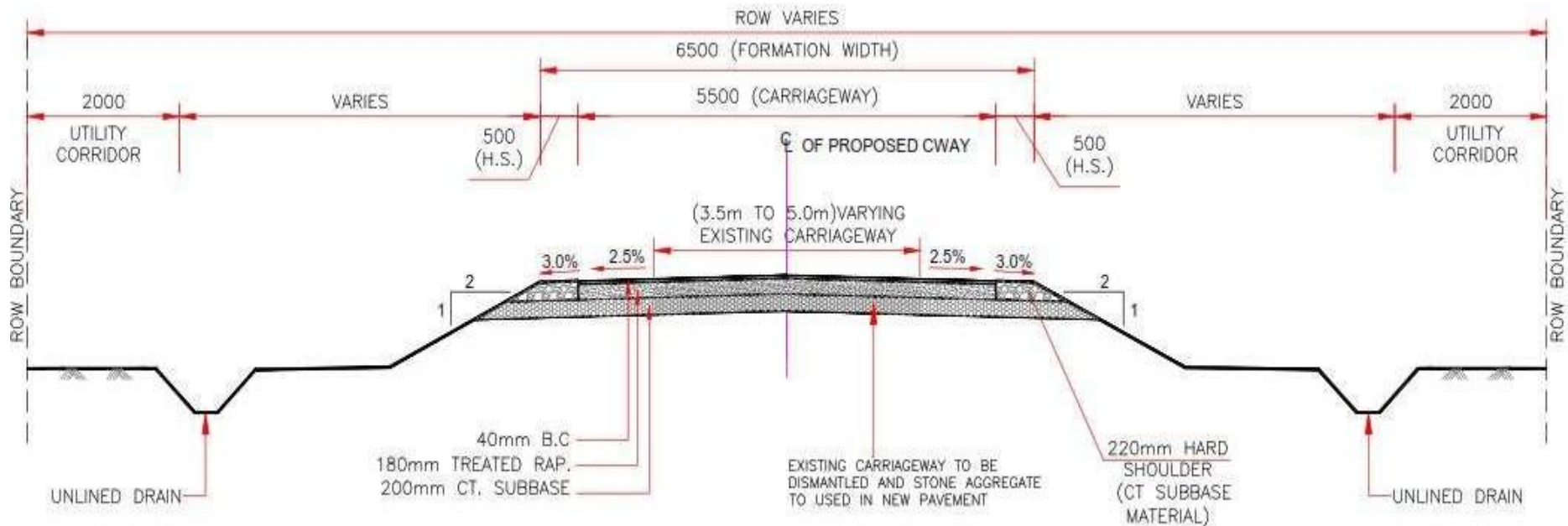


Appendix-B-I

## TYPE-II

\* Retaining wall and Breast wall shown in TCS II drawing is typical. Location of these components should be applied as per site condition.

### Appendix-B-I

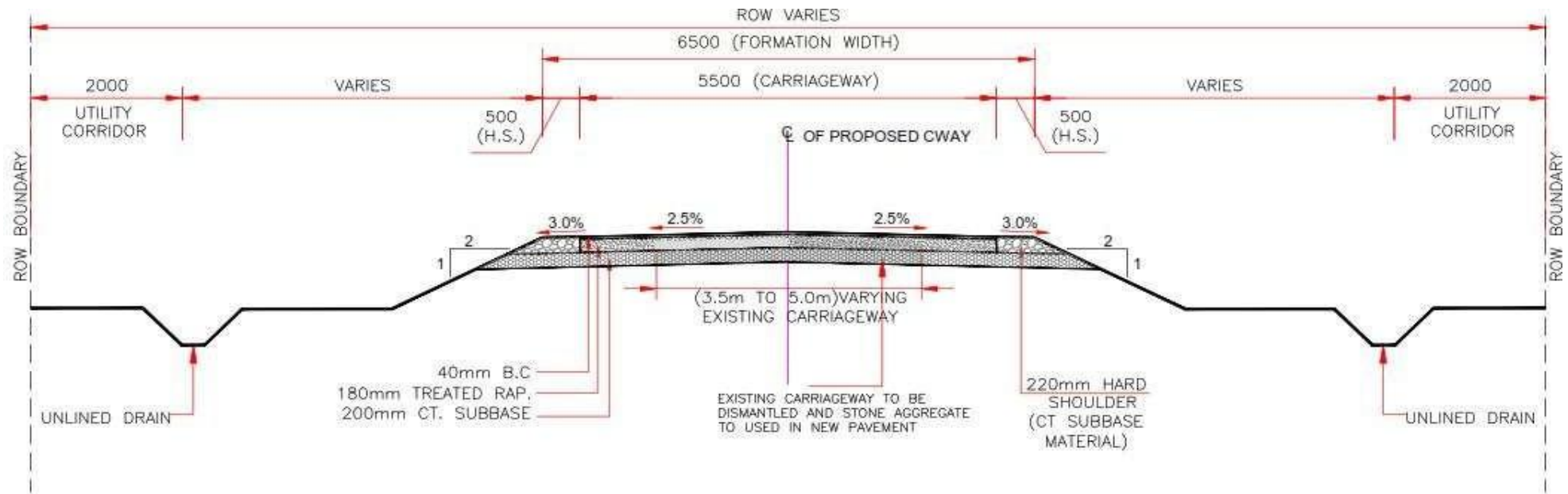


REHABILITATION IN RURAL SECTION (CUTTING)

## TYPE-III

\*\* TCS III is proposed for cutting in rural section and drawing shown for the same is typical. Wherever required the breast wall must be provided, if there is cutting in hill.

### Appendix-B-I



REHABILITATION IN RURAL SECTION (FILLING)

## TYPE-IV

\*\*\* TCS IV is proposed for filling in rural section and drawing shown for the same is typical. Wherever required the retaining wall must be provided, to confine the toe within ROW.

## **SCHEDULE - C**

*(See Clause 2.1)*

### **PROJECT FACILITIES**

#### **1. Project Facilities**

The Contractor shall construct the Project Facilities in accordance with the provisions of this Agreement. Such Project Facilities shall include:

- a) Roadside furniture;
- b) Pedestrian facilities;
- c) Bus shelter
- d) Passing Places
- e) Metal Beam Crash Barrier
- f) Breast Wall & Retaining Wall

Others to be specified

#### **2. Description of Project Facilities**

Each of the Project Facilities is described below:

**a) Roadside furniture;**

The roadside furniture shall include the provision of:

**i. Traffic Signs:**

Traffic signs include roadside signs, overhead signs and kerb-mounted signs along the entire Project Highway as per the manual of specifications.

**ii. Pavement Markings:**

Pavement markings shall cover road marking as per the manual of specifications.

**iii. LED Traffic Blinkers:**

LED Traffic Blinkers for the entire project highway at the locations as suggested in Manual.

**iv. Crash barrier**

As per clause 9.4 of IRC:SP-73 and as per details given in schedule-B

**v. Delineators**

Delineators for the entire Project Highway at the locations as suggested in Manual.

**vi. Hectometre / Kilometre stones:**

Hectometre/ Kilometre Stones for the entire Project Highway at the locations as suggested in Manual.

**vii) Road Studs:**

Reflective Road studs along the entire Project Highway as per the manual of specifications.

**b) Pedestrian facilities;**

The pedestrian facilities shall be provided as per the Manual.

**c) Bus Shelter**

The Contractor shall provide additional 8 nos. of Bus Shelters along the project highway and the locations are given below. The design of Bus Shelters should be aesthetically pleased with surrounding. The locations of these bus shelters shall be finalized by the Contractor in consultation with Authority's Engineer.

S. No.	Design Chainage	Side
1	47+850	Left
2	48+550	Right
3	51+105	Left
4	52+310	Right
5	53+380	Left
6	54+700	Right
7	57+370	Left
8	58+060	Right

## **SCHEDULE – D**

*(See Clause 2.1)*

### **SPECIFICATIONS AND STANDARDS**

#### **1. Construction**

The Contractor shall comply with the Specifications and Standards set forth in Annex-I of this Schedule-D for construction of the Project Highway.

#### **2. Design Standards**

The Project Highway including Project Facilities shall conform to design requirements set out in the following documents:

Manual of Specifications and Standards for Two-Laning of Highways (IRC:SP:73-2015), referred to herein as the Manual.

---

**Annex - I**

**(Schedule-D)**

**Specifications and Standards for Construction**

**1. Specification and Standards**

All Materials, works and construction operations shall conform to the Manual of Specifications and Standards for Two-Lanning of Highways (IRC:SP:73-2015), referred to as the Manual, and MORTH Specifications for Road and Bridge Works. Where the specification for a work is not given, Good Industry Practice shall be adopted to the satisfaction of the Engineer in charge.

**2. Deviations from the Specifications and Standards**

- (i) The terms “Concessionaire”, “Independent Engineer” and “Concession Agreement” used in the Manual shall be deemed to be substituted by the terms “Contractor”, “Authority’s Engineer” and “Agreement” respectively.
- (ii) [Notwithstanding anything to the contrary contained in Paragraph 1 above, the following Specifications and Standards shall apply to the Project Highway, and for purposes of this Agreement, the aforesaid Specifications and Standards shall be deemed to be amended to the extent set forth below:

<b>S. No.</b>	<b>Clause Referred in Manual</b>	<b>Item</b>	<b>Provisions as per Manual</b>	<b>Modified Provision</b>
1	2.2.1	Design Speed	80 kmph (min. speed for plain/rolling terrain)	Design speed has not been as per Manual to restrict the construction within the available ROW

[Note 1: Deviations from the aforesaid Specifications and Standards shall be listed out here. Such deviations shall be specified only if they are considered essential in view of project-specific requirements.]

## **SCHEDULE – E**

*(See Clause 2.1 and 14.2)*

### **MAINTENANCE REQUIREMENTS**

#### **1. Maintenance Requirements**

- (i) The Contractor shall, at all-time maintain the Project Highway in accordance with the provisions of this Agreement, Applicable Laws and Applicable Permits.
- (ii) The Contractor shall repair or rectify any Defect or deficiency set forth in Paragraph 2 of this Schedule-E within the time limit specified therein and any failure in this behalf shall constitute non-fulfillment of the Maintenance obligations by the Contractor. Upon occurrence of any breach hereunder, the Authority shall be entitled to effect reduction in monthly lump sum payment as set forth in Clause 14.6 of this Agreement, without prejudice to the rights of the Authority under this Agreement, including Termination thereof.
- (iii) All Materials, works and construction operations shall conform to the MORTH Specifications for Road and Bridge Works, and the relevant IRC publications. Where the specifications for a work are not given, Good Industry Practice shall be adopted..

#### **2. Repair/rectification of Defects and deficiencies**

The obligations of the Contractor in respect of Maintenance Requirements shall include repair and rectification of the Defects and deficiencies specified in Annex-I of this Schedule-E within the time limit set forth therein.

#### **3. Other Defects and deficiencies**

In respect of any Defect or deficiency not specified in Annex-I of this Schedule-E, the Authority's Engineer may, in conformity with Good Industry Practice, specify the permissible limit of deviation or deterioration with reference to the Specifications and Standards, and any deviation or deterioration beyond the permissible limit shall be repaired or rectified by the Contractor within the time limit specified by the Authority's Engineer.

**4. Extension of time limit**

Notwithstanding anything to the contrary specified in this Schedule-E, if the nature and extent of any Defect or deficiency justifies more time for its repair or rectification than the time specified herein, the Contractor shall be entitled to additional time in conformity with Good Industry Practice. Such additional time shall be determined by the Authority's Engineer and conveyed to the Contractor and the Authority with reasons thereof;

**5. Emergency repairs/restoration**

Notwithstanding anything to the contrary contained in this Schedule-E, if any Defect, deficiency or deterioration in the Project Highway poses a hazard to safety or risk of damage to property, the Contractor shall promptly take all reasonable measures for eliminating or minimizing such danger.

**6. Daily inspection by the Contractor**

The Contractor shall, through its engineer, undertake a daily visual inspection of the Project Highway and maintain a record thereof in a register to be kept in such form and manner as the Authority's Engineer may specify. Such record shall be kept in safe custody of the Contractor and shall be open to inspection by the Authority and the Authority's Engineer at any time during office hours.

**7. Pre-monsoon inspection / Post-monsoon inspection**

The Contractor shall carry out a detailed pre-monsoon inspection of all bridges, culverts and drainage system before [1st June] every year in accordance with the guidelines contained in IRC: SP:35. Report of this inspection together with details of proposed maintenance works as required on the basis of this inspection shall be sent to the Authority's Engineer before the [10th June] every year. The Contractor shall complete the required repairs before the onset of the monsoon and send to the Authority's Engineer a compliance report. Post monsoon inspection shall be done by the [30th September] and the inspection report together with details of any damages observed and proposed action to remedy the same shall be sent to the Authority's Engineer.

**8. Repairs on account of natural calamities**

All damages occurring to the Project Highway on account of a Force Majeure Event or wilful default or neglect of the Authority shall be undertaken by the Authority at its own cost. The Authority may instruct the Contractor to undertake the repairs at the rates agreed between the Parties.

**Annex – I**  
**(Schedule-E)**

**Repair/rectification of Defects and deficiencies**

The Contractor shall repair and rectify the defects and deficiencies specified in this Annex-I of Schedule-E within the time limit set forth in the table below.

<b>Nature of Defect or deficiency</b>		<b>Time limit for repair/ rectification</b>
<b>ROADS</b>		
<b>(a)</b>	<b>Carriageway and paved shoulders</b>	
(i)	Breach or blockade	Temporary restoration of traffic within 24 hours; permanent restoration within 15 (fifteen) days
(ii)	Any significant change in roughness value from original value [more than 5%] in a stretch of 1 km (as measured by a Calibrated bump integrator)	120 (one hundred and twenty) days
(iii)	Pot holes	24 hours
(iv)	Any cracks in road surface	15 (fifteen) days
(v)	Any depressions, rutting exceeding 10 mm in road surface	30 (Thirty) days
(vi)	Skidding	7 (seven) days
(vii)	Any other defect/distress on the road	15 (fifteen) days
(viii)	Damage to pavement edges	15 (fifteen) days
(ix)	Removal of debris, dead animals	6 hours
(x)	Any other defects/deficiency not covered above but pointed out by Engineer	3 (Three) days
<b>(b)</b>	<b>Granular earth shoulders, side slopes, drains and culverts</b>	

**Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)**

(i)	Edge drop at shoulders exceeding 40 mm	7 (Seven) days
(ii)	Variation by more than 1% in the prescribed slope of camber/cross fall (shall not be less than the camber on the main carriageway)	7 (seven) days
(iii)	Variation by more than 15% in the prescribed side (embankment) slopes	30 (thirty) days
(iv)	Rain cuts/gullies in slope	7 (Seven) days
(v)	Damage to or silting of culverts and side drains	7 (Seven) days
(vi)	Desilting of drains in urban/semi-urban areas	24 hours
(vii)	Railing, parapets, crash barriers	7 (Seven) days (Restore immediately if causing safety hazard)
(viii)	Any other defects/deficiency not covered above but pointed out by Engineer	3 (Three) days
<b>(c)</b>	<b>Road side furniture including road sign and pavement marking</b>	
(i)	Damage to shape or position, poor visibility or loss of retro-reflectivity	48 hours
(ii)	Painting of KM stone, railing, parapets, crash barriers	As and when required/Once every year
(iii)	Damaged/missing road signs required replacement	7 (Seven) days
(iv)	Damage to road mark ups	7 (Seven) days
(v)	Any other defects/deficiency not covered above but pointed out by Engineer	3 (Three) days
<b>(d)</b>	<b>Road lighting</b>	
(i)	Any major failure of the system	24 hours
(ii)	Faults and minor failures	8 hours
(iii)	Any other defects/deficiency not covered above but pointed out by Engineer	3 (Three) days
<b>(e)</b>	<b>Trees and plantation</b>	
(i)	Obstruction in a minimum head-room of 5 m	24 hours

**Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)**

	above carriageway or obstruction in visibility of road signs	
(ii)	Removal of fallen trees from carriageway	4 hours
(iii)	Deterioration in health of trees and bushes	Timely watering and treatment
(iv)	Trees and bushes requiring replacement	30 (Thirty) days
(v)	Removal of vegetation affecting sight line and road structures	15 (fifteen) days
(vi)	Any other defects/deficiency not covered above but pointed out by Engineer	3 (Three) days
<b>(f)</b>	<b>Other Project Facilities, Rest Area and Approach roads</b>	
(i)	Damage in pedestrian facilities, truck lay-buys, bus-bays, bus-shelters, cattle crossings, [Traffic Aid Posts, Medical Aid Posts ] and service roads	15 (fifteen) days
(ii)	Cleaning of toilets	Every 4 hours
(iii)	Defects in electrical, water and sanitary installations	24 hours
(iv)	Any other defects/deficiency not covered above but pointed out by Engineer	3 (Three) days
(v)	Rescue operations and attendance at accidents	Round the clock patrolling Inform police and other agencies immediately Removal of vehicles or debris. Assistance for first-aid and transport of accident victim to hospital Arrangement for safe movement of traffic
(vi)	Any other defects/deficiency not covered above but pointed out by Engineer	3 (Three) days
(vii)	Damaged vehicles or debris on the road	4 (Four) hours
(viii)	Malfunctioning of the mobile cranes	4 (four) hours

<b>Bridges</b>		
<b>(a)</b>	<b>Superstructure</b>	
(i)	Any damage, cracks, spalling/scaling  Temporary measures  Permanent measures	Within 48 hours  Within 15 (fifteen) days or as specified by the Authority's Engineer
<b>(b)</b>	<b>Bearings (metallic) of bridges</b>	
(i)	Deformation	15 (fifteen) days  Greasing of metallic bearings once in a year
<b>(c)</b>	<b>Joints</b>	
(i)	malfunctioning of joints	15 (fifteen) days
(ii)	Any other defects/deficiency not covered above (a) , (b) &(c) but pointed out by Engineer	3 (Three) days
<b>(d)</b>	<b>Foundations</b>	
(i)	Scouring and/or cavitation	15 (fifteen) days
<b>(e)</b>	<b>Piers, abutments, return walls and wing walls</b>	
(i)	Cracks and damages including settlement and tilting, Spalling, scaling	30 (thirty) days
(ii)	Any other defects/deficiency not covered above (d) & (e) but pointed out by Engineer	3 (Three) days
<b>(f)</b>	<b>Other items</b>	
(i)	Deforming of pads in elastomeric bearings	7 (seven) days
(ii)	Gathering of dirt in bearings and joints; or clogging of spouts, weep holes and vent-holes	3 (three) days
(iii)	Damage or deterioration in kerbs, parapets, handrails and crash barriers	3 (three) days

**Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)**

		(immediately within 24 hours if posing danger of safety)
(iv)	Rain-cuts or erosion of banks of the side slopes of approaches	7 (seven) days
(v)	Damage to wearing coat	15 (fifteen) days
(vi)	Damage or deterioration in approach Slabs, pitching, apron, toes, floor or guide bunds	30 (thirty) days
(vii)	Growth of vegetation affecting the structure or obstructing the waterway	15 (fifteen) days
(viii)	Any other defects/deficiency not covered above but pointed out by Engineer	3 (Three) days

The failure to address above measures for any of the defects/deficiency may attract reduction in payment as per schedule M

## **Schedule-F**

(See Clause 3.1.5(a))

### **APPLICABLE PERMITS**

#### **1. Applicable Permits**

The Contractor shall obtain, as required under the Applicable Laws, the following Applicable Permits:

- (a) Permission of the State Government for extraction of boulders from quarry;
- (b) Permission of Village Panchayat and Pollution Control Board for installation of crushers;
- (c) License for use of explosives;
- (d) Permission of the State Government for drawing water from river/reservoir;
- (e) License from inspector of factories or other competent Authority for setting up batching plant;
- (f) Clearance of Pollution Control Board for setting up batching plant;
- (g) Clearance of Village Panchayats and Pollution Control Board for setting up asphalt plant;
- (h) Permission of Village Panchayats and State Government for borrow earth; and
- (i) Any other permits, clearances or approvals required under Applicable Laws.

1.2 Applicable permits, as required, relating to environmental protection and conservation shall have been produced by the Authority in accordance with the provisions of this Agreement

## Schedule-G

(See Clause 7.1.1, 7.5.3 and 19.2)

### FORM OF BANK GUARANTEE

#### Annex-I

(See Clause 7.1.1)

### PERFORMANCE SECURITY

**The Managing Director,  
NHIDCL,  
3<sup>rd</sup> Floor, PTI Building, Sansad Marg,  
New Delhi**

WHEREAS:

- (A) \_\_\_\_\_ [name and address of contractor] (hereinafter called “the Contractor”) and [NHIDCL], (“**the Authority**”) have entered into an agreement (the “**Agreement**”) for “Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands through Engineering, Procurement & Construction (EPC) Basis Contract”, subject to and in accordance with the provisions of the Agreement.
- (B) The Agreement requires the Contractor to furnish a Performance Security for due and faithful performance of its obligations, under and in accordance with the Agreement, during the Construction Period and Defects Liability Period and maintenance period (as defined in the Agreement) in a sum of Rs. .... Crore (Rupees .... Crore) (the “Guarantee Amount”).
- (C) We, ..... through our branch at ..... (the “Bank”) have agreed to furnish this bank guarantee (hereinafter called the “Guarantee”) by way of Performance Security.

NOW, THEREFORE, the Bank hereby, unconditionally and irrevocably, guarantees and affirms as follows:

1. The Bank hereby unconditionally and irrevocably guarantees the due and faithful performance of the Contractor's obligations during and under and in accordance with the Agreement, and agrees and undertakes to pay to the Authority, upon its mere first written demand, and without any demur, reservation, recourse, contest or protest, and without any reference to the Contractor, such sum or sums up to an aggregate sum of the guarantee amount as the Authority shall claim, without the Authority being required to prove or to show grounds or reasons for its demand and/or for the sum specified therein.
2. A letter from the Authority, under the hand of an officer not below the rank of [Executive Director, NHIDCL], that the Contractor has committed default in the due and faithful performance of all or any of its obligations under and in accordance with the Agreement shall be conclusive, final and binding on the Bank. The Bank further agrees that the Authority shall be the sole judge as to whether the Contractor is in default in due and faithful performance of its obligations during and under the Agreement and its decision that the Contractor is in default shall be final, and binding on the Bank, notwithstanding any difference between the Authority and the Contractor, or any dispute between them pending before any court, tribunal, arbitrators or any other Authority or body, or by the discharge of the Contractor for any reason whatsoever.
3. In order to give effect to this Guarantee, the Authority shall be entitled to act as if the Bank were the principal debtor and any change in the constitution of the Contractor and/or the Bank, whether by their absorption with any other body or corporation or otherwise, shall not in any way or manner affect the liability or obligation of the Bank under this Guarantee.
4. It shall not be necessary, and the Bank hereby waives any necessity, for the Authority to proceed against the Contractor before presenting to the Bank its demand under this Guarantee.
5. The Authority shall have the liberty, without affecting in any manner the liability of the Bank under this Guarantee, to vary at any time, the terms and conditions of the Agreement or to extend the time or period for the compliance with, fulfillment and/or performance of all or any of the obligations of the Contractor contained in the Agreement or to postpone for any time, and from time to time, any of the rights and powers exercisable by the Authority against the Contractor, and either to enforce or forbear from enforcing any of the terms and conditions contained in the Agreement and/or the securities available to the Authority, and the Bank shall not be released from its liability and obligation under these presents by any exercise by the Authority of the liberty with reference to the matters aforesaid or by reason of time being given to the Contractor or any other forbearance, indulgence, act or omission on the part of

the Authority or of any other matter or thing whatsoever which under any law relating to sureties and guarantors would but for this provision have the effect of releasing the Bank from its liability and obligation under this Guarantee and the Bank hereby waives all of its rights under any such law.

6. This Guarantee is in addition to and not in substitution of any other guarantee or security now or which may hereafter be held by the Authority in respect of or relating to the Agreement or for the fulfillment, compliance and/or performance of all or any of the obligations of the Contractor under the Agreement.
7. Notwithstanding anything contained hereinbefore, the liability of the Bank under this Guarantee is restricted to the Guarantee amount and this Guarantee will remain in force for the period specified in paragraph 8 below and unless a demand or claim in writing is made by the Authority on the Bank under this Guarantee all rights of the Authority under this Guarantee shall be forfeited and the Bank shall be relieved from its liabilities hereunder.
8. The Performance Security shall cease to be in force and effect upto 90 (ninety) days after the end of the Defects Liability Period as set forth in Clause 17.1 of EPC agreement.
9. The Bank undertakes not to revoke this Guarantee during its currency, except with the previous express consent of the Authority in writing, and declares and warrants that it has the power to issue this Guarantee and the undersigned has full powers to do so on behalf of the Bank.
10. Any notice by way of request, demand or otherwise hereunder may be sent by post addressed to the Bank at its above referred branch, which shall be deemed to have been duly authorized to receive such notice and to effect payment thereof forthwith, and if sent by post it shall be deemed to have been given at the time when it ought to have been delivered in due course of post and in proving such notice, when given by post, it shall be sufficient to prove that the envelope containing the notice was posted and a certificate signed by an officer of the Authority that the envelope was so posted shall be conclusive.  
[[[
11. This Guarantee shall come into force with immediate effect and shall remain in force and effect for up to the date specified in para 8 above or until it is released earlier by the Authority pursuant to the provisions of the Agreement.
12. This guarantee shall also be operable at our..... Branch at New Delhi, from whom, confirmation regarding the issue of this guarantee or extension/ renewal thereof shall be made available on demand. In the contingency of this guarantee being invoked and payment thereunder claimed, the said branch shall accept such invocation letter and make payment of amounts so demanded under the said invocation.

13. Intimation regarding issuance of this Bank Guarantee shall be sent to Authority's Bank through SFMS gateway as per the details below:

<b>S.No.</b>	<b>Particulars</b>	<b>Details</b>
1	Name of Beneficiary	National Highways & Infrastructure Development Corporation Limited
2	Beneficiary Bank Account No.	90621010002659
3	Beneficiary Bank Branch	IFSC SYNB0009062
4	Beneficiary Bank Branch Name	Transport Bhawan, New Delhi
5	Beneficiary Bank Address	Syndicate Bank transport Bhawan, 1st Parliament Street, New Delhi-110001

Signed and sealed this ..... day of ..... 20..... at .....

SIGNED, SEALED AND DELIVERED

For and on behalf of the Bank by:

(Signature)

(Name)

(Designation)

(Code Number)

(Address)

NOTES:

- (i) The bank guarantee should contain the name, designation and code number of the officer(s) signing the guarantee.
- (ii) The address, telephone number and other details of the head office of the Bank as well as of issuing branch should be mentioned on the covering letter of issuing branch.

Annex-II

(Schedule-G)

(See Clause 19.2)

**Form for Guarantee for Advance Payment**

**The Managing Director,  
NHIDCL,  
3<sup>rd</sup> Floor, PTI Building, Sansad Marg,  
New Delhi**

WHEREAS:

- (A) [name and address of contractor] (hereinafter called “**the Contractor**”) has executed an agreement (hereinafter called the “Agreement”) with the [NHIDCL], (hereinafter called “**the Authority**”) for the “Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands through Engineering, Procurement & Construction (EPC) Basis Contract”, subject to and in accordance with the provisions of the Agreement.
- (B) In accordance with Clause 19.2 of the Agreement, the Authority shall make to the Contractor an interest bearing @Bank Rate + 3% advance payment (herein after called “Advance Payment”) equal to 10% (ten per cent) of the Contract Price; and that the Advance Payment shall be made in two installments subject to the Contractor furnishing an irrevocable and unconditional guarantee by a scheduled bank for an amount equivalent to 110% (one hundred and ten percent) of such installment to remain effective till the complete and full repayment of the installment of the Advance Payment as security for compliance with its obligations in accordance with the Agreement. The amount of {first/second} installment of the Advance Payment is Rs.            cr. (Rupees        crore) and the        amount of this Guarantee is Rs.            cr. (Rupees            crore) (the “Guarantee Amount”)
- (C) We, ..... through our branch at ..... (the “Bank”) have agreed to furnish this bank guarantee (hereinafter called the “Guarantee”) for the Guarantee Amount.

NOW, THEREFORE, the Bank hereby, unconditionally and irrevocably, guarantees and

affirms as follows:

1. The Bank hereby unconditionally and irrevocably guarantees the due and faithful repayment on time of the aforesaid instalment of the Advance Payment under and in accordance with the Agreement, and agrees and undertakes to pay to the Authority, upon its mere first written demand, and without any demur, reservation, recourse, contest or protest, and without any reference to the Contractor, such sum or sums up to an aggregate sum of the Guarantee Amount as the Authority shall claim, without the Authority being required to prove or to show grounds or reasons for its demand and/or for the sum specified therein.  
A letter from the Authority, under the hand of an officer not below the rank of [General Manager in the National Highways Authority of India], that the Contractor has committed default in the due and faithful performance of all or any of its obligations for the repayment of the instalment of the Advance Payment under and in accordance with the Agreement shall be conclusive, final and binding on the Bank. The Bank further agrees that the Authority shall be the sole judge as to whether the Contractor is in default in due and faithful performance of its obligations during and under the Agreement and its decision that the Contractor is in default shall be final and binding on the Bank, notwithstanding any differences between the Authority and the Contractor, or any dispute between them pending before any court, tribunal, arbitrators or any other authority or body, or by the discharge of the Contractor for any reason whatsoever
2. In order to give effect to this Guarantee, the Authority shall be entitled to act as if the Bank were the principal debtor and any change in the constitution of the Contractor and/or the Bank, whether by their absorption with any other body or corporation or otherwise, shall not in any way or manner affect the liability or obligation of the Bank under this Guarantee.
3. It shall not be necessary, and the Bank hereby waives any necessity, for the Authority to proceed against the Contractor before presenting to the Bank its demand under this Guarantee.
4. The Authority shall have the liberty, without affecting in any manner the liability of the Bank under this Guarantee, to vary at any time, the terms and conditions of the Advance Payment or to extend the time or period of its repayment or to postpone for any time, and from time to time, any of the rights and powers exercisable by the Authority against the Contractor, and either to enforce or forbear from enforcing any of the terms and conditions contained in the Agreement and/or the securities available to the Authority, and the Bank shall not be released from its liability and obligation under these presents by any exercise by the Authority of the liberty with reference to the matters aforesaid or by reason of time being given to the Contractor or any other forbearance, indulgence, act or omission on the part of the Authority or of any other matter or thing whatsoever which under any law relating to sureties and guarantors would but for this provision have the effect of releasing the Bank from its liability and

obligation under this Guarantee and the Bank hereby waives all of its rights under any such law.

5. This Guarantee is in addition to and not in substitution of any other guarantee or security now or which may hereafter be held by the Authority in respect of or relating to the Advance Payment.
6. Notwithstanding anything contained hereinbefore, the liability of the Bank under this Guarantee is restricted to the Guarantee amount and this Guarantee will remain in force for the period specified in paragraph 8 below and unless a demand or claim in writing is made by the Authority on the Bank under this Guarantee all rights of the Authority under this Guarantee shall be forfeited and the Bank shall be relieved from its liabilities hereunder.
7. The guarantee shall cease to be in force and effect 90 (ninety) days after the end of the one year from the date of payment of the installment of the Advance Payment, as set forth in Clause 19.2 of the Agreement.
8. The Bank undertakes not to revoke this Guarantee during its currency, except with the previous express consent of the Authority in writing, and declares and warrants that it has the power to issue this Guarantee and the undersigned has full powers to do so on behalf of the Bank.
9. Any notice by way of request, demand or otherwise hereunder may be sent by post addressed to the Bank at its above referred branch, which shall be deemed to have been duly authorized to receive such notice and to effect payment thereof forthwith, and if sent by post it shall be deemed to have been given at the time when it ought to have been delivered in due course of post and in proving such notice, when given by post, it shall be sufficient to prove that the envelope containing the notice was posted and a certificate signed by an officer of the Authority that the envelope was so posted shall be conclusive.
10. This Guarantee shall come into force with immediate effect and shall remain in force and effect for up to the date specified in para 8 above or until it is released earlier by the Authority pursuant to the provisions of the Agreement.
11. This guarantee shall also be operable at our..... Branch at New Delhi, from whom, confirmation regarding the issue of this guarantee or extension/ renewal thereof shall be made available on demand. In the contingency of this guarantee being invoked and payment thereunder claimed, the said branch shall accept such invocation letter and make payment of amounts so demanded under the said invocation.
12. Intimation regarding issuance of this Bank Guarantee shall be sent to Authority's Bank through SFMS gateway as per the details below:

S.No.	Particulars	Details
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Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-VI)

1	Name of Beneficiary	National Highways & Infrastructure Development Corporation Limited
2	Beneficiary Bank Account No.	90621010002659
3	Beneficiary Bank Branch	IFSC SYNB0009062
4	Beneficiary Bank Branch Name	Transport Bhawan, New Delhi
5	Beneficiary Bank Address	Syndicate Bank transport Bhawan, 1st Parliament Street, New Delhi-110001

Signed and sealed this ..... day of ..... 20..... at .....

SIGNED, SEALED AND DELIVERED

For and on behalf of the Bank by:

(Signature)

(Name)

(Designation)

(Code Number)

(Address)

### Schedule-H

(See Clauses 10.1.4 and 19.3)

#### Contract Price Weightages

1. (i) The Contract Price for this Agreement is **Rs.      Crore.**

1. (ii) Proportions of the Contract Price for different stages of Construction of the Project Highway shall be as specified below:

Item	Weightage in percentage to the Contract Price	Stage for Payment	Percentage weightage
1	2	3	4
Road works including culverts, widening and repair of culverts	82.87%	<b>B.1- Reconstruction/ New intermediate lane//bypass (Flexible pavement)</b>	
		(1) Site Clearance & Dismantling	2.05%
		(2) Sub-base	24.86%
		(3) Hard Shoulder	2.87%
		(4) Treated RAP/RAP/Bituminous Base course	30.83%
		(5) Bituminous Wearing course	11.93%
		<b>D-Re-Construction and New culverts on existing road, realignments, bypasses:</b>	
		Culverts (lengths < 6m)	27.28%
		Culverts maintenance of 15 culvert as per schedule B	0.18%
Minor Bridges/	0.05%	<b>A.1- Widening and Repair of Minor bridges (length &gt;6 m and &lt; 60 m)</b>	

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

Underpasses/ Overpasses		Bridge Repair as per schedule B	100%	
		<b>A.2- New Minor bridges (length &gt;6 m and &lt; 60 m)</b>		
		<b>(1) Foundation + Sub- Structure:</b> On completion of the foundation work including foundations for wing and return walls, abutments, piers upto the abutment/ pier cap.	0%	
		<b>(2) Super-structure:</b> On completion of the super-structure in all respects including wearing coat, bearings, expansion joints, hand rails, crash barriers, road signs & markings, tests on completion in all respect.	0%	
		<b>(3) Approaches:</b> On completion of approaches including Retaining walls, stone pitching, protection works complete in all respect and fit for use.	0%	
Major Bridge works	0.00%	<b>A.1- Widening and Repair of Major bridges</b>		
		Major Bridges	0.00%	
		<b>A.2 -New major bridges &amp; Viaduct</b>		
		(1) Foundation	0.00%	
		(2) Sub-structure	0.00%	
		(3) Super-structure (including bearings)	0.00%	
		(4) Wearing Coat including expansion joints	0.00%	
		(5) Miscellaneous Items like hand rails, crash barriers, road markings etc.	0.00%	
		(6) Wing walls/ return walls	0.00%	
		(7) Guide Bunds, River Training works etc.	0.00%	
		(8) Approaches (including Retaining walls, stone pitching and protection works)	0.00%	
Other works	17.07%	(i) Protection Work		
			(a) Breast Wall	4.40%
			(b) Retaining Wall	45.05%
		<b>(ii) Drainage</b>		
			(a) Unlined Drains	0.45%

		(b) Lined Random Rubble Masonry Drains	1.94%
		(iii) Traffic Sign, Marking and other Appurtenances	31.02%
		(iv) Junctions	2.95%
		(v) Project facilities	
		(a) Bus Shelters	6.40%
		(b) Passing Places	5.47%
		(c) Truck lay-byes	0.000%
		(d) Rest areas	0.000%
		(e) others	0.000%
		(vi) Repair of Protection Works other than approaches to the bridges, elevated sections/ flyover/ grade separators and ROBs/ RUBs	0.00%
		(vii) Site Clearance & Dismantling	0.00%
		(viii) Safety and traffic management during construction	0.00%
		(IX) Pre mix carpeting with seal coat for filling of pot holes and repair as per Schedule B	2.32%

**Procedure of estimating the value of work done.**

(i) Road works.

Procedure for estimating the value of road work done shall be as follows:

**Table 1.3.1**

Stage for Payment	Percentage weightage	Payment Procedure
<b>B.1- Reconstruction/ New 4-lane realignment/bypass (Flexible pavement)</b>		
(1) Site Clearance & Dismantling	2.05%	Unit of measurement is linear length. Payment of each stage shall be made on pro rata basis on completion of a stage in a length of not less than 10 (ten) percent of the total length.
(2) Sub-base	24.86%	
(3) Hard Shoulder	2.87	
(4) Treated RAP/RAP/Bituminous Base course	30.83%	

(5) Bituminous wearing course	11.93%	
<b>D-Re-construction and New culverts on existing road, realignments, bypasses:</b>		
(1) Culverts (length < 6m)	27.28%	Cost of ten completed culverts shall be determined on pro rata basis with respect to the total number of culverts. Payment shall be made on the completion of at least two culverts
Culverts maintenance of 15 culvert as per schedule B	0.18%	

@ For example, if the total length of bituminous work to be done is 100 km, the cost per km of bituminous work shall be determined as follows:

$$\text{Cost per km} = P \times \text{weightage for road work} \times \text{weightage for bituminous work} \times (1/L)$$

Where P = Contract Price

$$L = \text{Total length in km}$$

Similarly, the rates per km for stages shall be worked out accordingly.

**Note: The length affected due to law and order problems or litigation during execution due to which the Contractor is unable to execute the work, may be deducted from the total project length for payment purposes. The total length calculated here is only for payment purposes and will not affect and referred in other clauses of the Contract Agreement.**

### 1.1.1 Minor Bridge and Underpasses/ Overpasses

Procedure for estimating the value of Minor Bridge and Underpasses/ Overpasses shall be as stated in table 1.3.2:

**Table 1.3.2**

Stage of Payment	Weightage	Payment Procedure
1	2	3
<b>A.1- Widening and Repair of Minor bridges (length &gt;6 m and &lt;</b>	100%	Cost of each minor bridge shall be determined on pro rata basis with respect to the total linear

60 m)		length of the minor bridges. Payment shall be made on the completion of widening & repair works of a minor bridge.
<b>A.2- <u>New minor bridges</u></b>		
<b>(1) Foundation + Sub- Structure:</b> On completion of the foundation work including foundations for wing and return walls, abutments, piers upto the abutment/ pier cap.	0%	Cost of each minor bridge shall be determined on pro rata basis with respect to the total linear length (m) of the minor bridges. Payment against foundation + sub-structure shall be made on pro-rata basis on completion of a stage i.e. not less than 25% of the scope of foundation + sub-structure of each bridge subject to completion of at least two foundations along with sub-structure upto abutment/pier cap level of each bridge. In case where load testing is required for foundation, the trigger of first payment shall include load testing also where specified.
<b>(2) Super-structure:</b> On completion of the super-structure in all respects including wearing coat, bearings, expansion joints, hand rails, crash barriers, road signs & markings, tests on completion in all respect.	0%	Payment shall be made on pro-rata basis on completion of a stage i.e. completion of super-structure of at least one span in all respects as specified in the column of “Stage of Payment” in this sub-clause.
<b>(3) Approaches:</b> On completion of approaches including Retaining walls, stone pitching, protection works complete in all respect and fit for use.	0%	Payment shall be made on pro-rata basis on completion of a stage i.e. completion of approaches in all respect as specified in the column of “Stage of Payment” in this sub-clause.

### 1.1.2 Major Bridge works & Viaducts

Procedure for estimating the value of Major Bridge works & Viaducts shall be as stated in table 1.3.3:

**Table 1.3.3**

Stage for Payment	Percentage weightage	Payment Procedure
<b>A.1- Widening and Repair of Major bridges</b>	0.00%	Cost of each major bridge shall be determined on pro rata basis with respect to the total linear length of the major bridges. Payment shall be made on the completion of widening & repair works of a minor bridge.
<b>A.2-<u>New major bridges &amp; Viaduct</u></b>		
(1) Foundation	0.000%	Cost of each major bridge/ Viaduct shall be determined on pro rata basis with respect to the total linear length (m) of the Major bridge/ Viaduct. Payment against foundation shall be made on pro-rata basis on completion of a stage i.e. not less than 25% of the scope of foundation of the Major bridge/ Viaduct subject to completion of at least two foundations of the Major bridge/ Viaduct. In case where load testing is required for foundation, the trigger of first payment shall include load testing also where specified.
(2) Sub-structure	0.000%	Payment against sub-structure shall be made on pro-rata basis on completion of a stage i.e. not less than 25% of the scope of sub-structure of the Major bridge/ Viaduct subject to completion of at least two sub-structures of abutments/piers upto abutment/pier cap level of the Major bridge/ Viaduct.
(3) Super-structure (including bearings)	0.000%	Payment shall be made on pro-rata basis on completion of a stage i.e. completion of super-structure including bearings of at least one span in all respects as specified.

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

(4) Wearing Coat including expansion joints	0.000%	Payment shall be made on completion of wearing coat including expansion joints complete in all respects as specified.
(5) Miscellaneous Items like hand rails, crash barriers, road markings etc.	0.000%	Payment shall be made on completion of all miscellaneous works like hand rails, crash barriers, road markings etc. complete in all respects as specified.
(6) Wing walls/ return walls	0.000%	Payment shall be made on completion of all wing walls/return walls complete in all respects as specified.
(7) Guide Bunds, River Training works etc.	0.000%	Payment shall be made on completion of all Guide Bunds/River Training works etc. complete in all respects as specified.
(8) Approaches (including Retaining walls, stone pitching and protection works)	0.000%	Payment shall be made on completion of both approaches including stone pitching, protection works, etc. complete in all respect as specified.

1.1.3 Other works.

Procedure for estimating the value of other works done shall be as stated in table 1.3.4:

**Table 1.3.4**

Stage for Payment	Percentage weightage	Payment Procedure
(i) Protection Work		Unit of measurement is linear length in km. Payment shall be made on pro-rata basis on completion of a stage in a length of not less than 10% (ten per cent) of the total length.
(a) Breast Wall	4.40%	
(b) Retaining Wall	45.05%	
(ii) Drainage		
(a) Unlined Drains	0.45%	
(b) Lined Random Rubble Masonry Drains	1.94%	

**Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)**

(ii) Traffic Sign, Marking and other Appurtenances	31.02%		
(iii) Junctions	2.95%	Payment shall be made on pro rata basis for completed facilities.	
(v) Project facilities	(i) Bus shelters	6.40%	Payment shall be made on pro-rata basis for completed facilities.
	(ii) Passing Places	5.47%	
	(ii) Truck lay bye	0.000%	
	(iii) Rest areas	0.000%	
	(iv) others	0.000%	
(vi) Repair of Protection Works other than approaches to the bridges, elevated sections/ flyover/ grade separators and ROB/ RUBs	0.000%	Unit of measurement is linear length. Payment shall be made on pro-rata basis on completion of a stage in a length of not less than 10% (ten per cent) of the total length.	
(vii) Site Clearance & Dismantling	0.00%		
(v) Safety and traffic management during construction	0.000%	Payment shall be made on pro rata basis every six months.	
(vii) Pre mix carpeting with seal coat for filling of pot holes and repair as per Schedule B	2.32%	Payment shall be made on pro-rata basis on completion of a stage in a length of not less than 5 Km. However the provisions are optional depending upon the site condition.	

**2. Procedure for payment for Maintenance**

- (a) The cost for maintenance shall be as stated in Clause 14.1.1.
- (b) Payment for Maintenance shall be made in Monthly basis in accordance with the provisions of Clause 19.6 & 19.7 of the Contract Agreement.

## **SCHEDULE-I**

(See Clause 10.2)

### **DRAWINGS**

#### **1. Drawings**

In compliance of the obligations set forth in Clause 10.2 of this Agreement, the Contractor shall furnish to the Authority's Engineer, free of cost, all Drawings listed in Annex-I of this Schedule-I.

#### **2. Additional Drawings**

If the Authority's Engineer determines that for discharging its duties and functions under this Agreement, it requires any drawings other than those listed in Annex-I, it may by notice require the Contractor to prepare and furnish such drawings forthwith. Upon receiving a requisition to this effect, the Contractor shall promptly prepare and furnish such drawings to the Authority's Engineer, as if such drawings formed part of Annex-I of this Schedule-I.

## **Annex-I**

(Schedule-I)

### **List of Drawings**

Alignment Plan and longitudinal Section are enclosed in digital form in CD marked as Annex-I

[Note: The Authority shall describe in this Annex-I, all the Drawings that the Contractor is required to furnish under Clause 10.2.]

- Typical Cross-section with details of pavement structures.

## **SCHEDULE-J**

(See Clause 10.3.2)

### **PROJECT COMPLETION SCHEDULE**

#### **1. Project Completion Schedule**

During Construction period, the Contractor shall comply with the requirements set forth in this Schedule J for each of the Project Milestones and the **Scheduled Completion Date**. Within 15 (fifteen) days of the date of each Project Milestone, the Contractor shall notify the Authority of such compliance along with necessary particulars thereof.

#### **2. Project Milestone-I**

(i) Project Milestone-I shall occur on the date falling on the 180<sup>th</sup> (One hundred Eighty) day from the Appointed Date (the “**Project Milestone-I**”).

(ii) Prior to the occurrence of Project Milestone-I, the Contractor shall have commenced construction of the Project Highway and submitted to the Authority duly and validly prepared Stage Payment Statements completion schedule in reference to Schedule-H Items, Stages and Sub-stages payment statements for an amount not less than 10% (ten per cent) of the Contract Price.

#### **3. Project Milestone-II**

(i) Project Milestone-II shall occur on the date falling on the 240<sup>th</sup> (Two hundred and forty) day from the Appointment Date (the “**Project Milestone-II**”).

(ii) Prior to the occurrence of Project Milestone-II, the Contractor shall have commenced construction of the Project Highway and submitted to the Authority duly and validly prepared Stage Payment Statements completion schedule in reference to Schedule-H Items, Stages and Sub-stages payment statements for an amount not less than 35% (thirty five per cent) of the Contract Price and should have started construction of all bridges.

#### **4. Project Milestone-III**

(i) Project Milestone-III shall occur on the date falling on the 300<sup>th</sup> (Threehundred) day from the Appointed Date (the “**Project Milestone-III**”).

(ii) Prior to the occurrence of Project Milestone-III, the Contractor shall have continued with construction of the Project Highway and submitted to the Authority duly and validly prepared payment Statements for an amount not less than 60% (sixty per cent) of the Contract Price and should have started the construction of all project facilities.

## **5 Schedule Completion Date**

(i) The Schedule Completion Date shall occur on the 365th (Three hundred and sixty fifth) day from the Appointed Date.

(ii) On or before the Scheduled Completion Date, the Contractor shall have completed construction in accordance with this Agreement.

## **6 Extension of time**

Upon extension of any or all of the aforesaid Project Milestones or the Scheduled Completion Date, as the case may be, under and in accordance with the provisions of this Agreement, the Project Completion Schedule shall be deemed to have been amended accordingly.

## **SCHEDULE-K**

(See Clause 12.1.2)

### **Tests on Completion**

#### **1. Schedule for Tests**

- (i) The Contractor shall, no later than 30 (thirty) days prior to the likely completion of construction, notify the Authority's Engineer and the Authority of its intent to subject the Project Highway to Tests, and no later than 10 (ten) days prior to the actual date of Tests, furnish to the Authority's Engineer and the Authority detailed inventory and particulars of all works and equipment forming part of Works.
- (ii) The Contractor shall notify the Authority's Engineer of its readiness to subject the Project Highway to Tests at any time after 10 (ten) days from the date of such notice, and upon receipt of such notice, the Authority's Engineer shall, in consultation with the Contractor, determine the date and time for each Test and notify the same to the Authority who may designate its representative to witness the Tests. The Authority's Engineer shall thereupon conduct the Tests itself or cause any of the Tests to be conducted in accordance with Article 12 and this Schedule K.

#### **2 Tests**

- (i) Visual and physical test: The Authority's Engineer shall conduct a visual and physical check of construction to determine that all works and equipment forming part thereof conform to the provisions of this Agreement. The physical tests shall include: all the tests specified in IRC code, manual and MORTH specifications for the road and Bridge works, 5th revision, 2013.

**Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)**

- (ii) Riding quality test: Riding quality of each lane of the carriageway shall be checked with the help of a Network Survey Vehicle (NSV) fitted with latest equipments and the maximum permissible roughness for purposes of this Test shall be [2,000 (two thousand)] mm for each kilometre.
- (iii) Tests for bridges: All major and minor bridges shall be subjected to the rebound hammer and ultrasonic pulse velocity tests, to be conducted in accordance with the procedure described in Special Report No. 17: 1996 of the IRC Highway Research Board on Nondestructive Testing Techniques, at two spots in every span, to be chosen at random by the Authority's Engineer. Bridges with a span of 15 (fifteen) meters or more shall also be subjected to load testing.
- (iv) Other tests: The Authority's Engineer may require the Contractor to carry out or cause to be carried additional tests, in accordance with Good Industry Practice, for determining the compliance of the Project Highway with Specifications and Standards, except tests as specified in clause 5, but shall include measuring the reflectivity of road markings and road signs; and measuring the illumination level (lux) of lighting using requisite testing equipment.
- (v) Environmental audit: The Authority's Engineer shall carry out a check to determine conformity of the Project Highway with the environmental requirements set forth in Applicable Laws and Applicable Permits.
- (vi) Safety Audit: The Authority's Engineer shall carry out or cause to be carried out, a safety audit to determine conformity of the Project Highway with the safety requirements and Good Industry Practice.

### **3 Agency for conducting Tests**

All Tests set forth in this Schedule-K shall be conducted by the Authority's Engineer or such other agency or person as it may specify in consultation with the Authority.

### **4. Completion Certificate**

Upon successful completion of Tests, the Authority's Engineer shall issue the Completion Certificate in accordance with the provisions of Article 12.

- 5. The Authority Engineer will carry out tests with following equipment at his own cost in the presence of contractor's representative.

Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)

Sr. No.	Key metrics of Asset	Equipment to be used	Frequency of condition survey
1	Surface Defects of pavement	Network Survey Vehicle (NSV)	At least twice a year (As per survey months defined for the state basis rainy season)
2	Roughness of pavement	Network Survey Vehicle (NSV)	At least twice a year (As per survey months defined for the state basis rainy season)
3	Strength of pavement	Falling Weight Deflectometer	At least once a year
4	Bridges	Mobile Bridge Inspection Unit (MBU)	At least twice a year (As per survey months defined for the state basis rainy season)
5	Road signs	Retro-reflectometer	At least twice a year (As per survey months defined for the state basis rainy season)

The first testing with the help of NSV shall be conducted at the time of issue of Completion Certificate.

## SCHEDULE-L

(See Clause 12.2 and 12.4)

### COMPLETION CERTIFICATE

1. I, ..... (Name of the Authority's Engineer), acting as Authority's Engineer, under and in accordance with the Agreement dated ..... (the "**Agreement**"), for construction of the "Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands through Engineering, Procurement & Construction (EPC) Basis Contract through ..... (Name of Contractor), hereby certify that the Tests in accordance with Article 12 of the Agreement have been successfully undertaken to determine compliance of the Project Highway with the provisions of the Agreement, and I am satisfied that the Project Highway can be safely and reliably placed in service of the Users thereof..
2. It is certified that, in terms of the aforesaid Agreement, all works forming part of Project Highway have been completed, and the Project Highway is hereby declared fit for entry into operation on this the .....day of 20 .....

SIGNED, SEALED AND DELIVERED

For and on behalf of  
Authority's Engineer by:

(Signature)

(Name)

(Designation)

(Address)

## SCHEDULE-M

(See Clauses 14.6., 15.2 and 19.7)

### PAYMENT REDUCTION FOR NON-COMPLIANCE

#### 1. Payment reduction for non-compliance with the Maintenance Requirements

(i) Monthly lump sum payments for maintenance shall be reduced in the case of non-compliance with the Maintenance Requirements set forth in Schedule-E.

(ii) Any deduction made on account of non-compliance with the maintenance Requirements shall not be paid even after compliance subsequently. The deduction shall continue to be made every month until compliance is done.

(iii) The Authority's Engineer shall calculate the amount of payment reduction on the basis of weightage in percentage assigned to non-conforming items as given in Paragraph 2.

#### 2. Percentage reductions in lump sum payments

(i) The following percentages shall govern the payment reduction:

S. No.	Item/Defect/Deficiency	Percentage
(a)	<b>Carriageway/Pavement</b>	
(i)	Potholes, cracks, other surface defects	15%
(ii)	Repairs of Edges, Rutting	5%
(b)	<b>Road, Embankment, Cuttings, Shoulders</b>	
(i)	Edge drop, inadequate crossfall, undulations, settlement, potholes, ponding, obstructions	10%
(ii)	Deficient slopes, raincuts, disturbed pitching, vegetation growth, pruning of trees	5%
(c)	<b>Bridges and Culverts</b>	
(i)	Desilting, cleaning, vegetation growth, damaged pitching, flooring, parapets, wearing course, footpaths, any damage to foundations	20%

**Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate Lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands on EPC basis (Package-5)**

(ii)	Any Defects in superstructures, bearings and sub-structures	10%
(iii)	Painting, repairs/replacement kerbs, railings, parapets, guideposts/crash barriers	5%
(d)	<b>Roadside Drains</b>	
(i)	Cleaning and repair of drains	5%
(e)	<b>Road Furniture</b>	
(i)	Cleaning, painting, replacement of road signs, delineators, road markings, 200 m/km/5th km stones	5%
(f)	<b>Miscellaneous Items</b>	
(i)	Removal of dead animals, broken down/accidented vehicles, fallen trees, road blockades or malfunctioning of mobile crane	10%
(ii)	Any other Defects in accordance with paragraph 1.	5%
(g)	<b>Defects in Other Project Facilities</b>	5%

(ii) The amount to be deducted from monthly lump-sum payment for non-compliance of particular item shall be calculated as under:

$$R = \frac{P}{100} \times (M1 \text{ or } M2) \times \frac{L1}{L}$$

Where,

P= Percentage of particular item/Defect/deficiency for deduction

M1= Monthly lump-sum payment in accordance para 1.2 above of this Schedule M2= Monthly lump-sum payment in accordance para 1.2 above of this Schedule

L1= Non-complying length L = Total length of the road,

L = Total length of the road,

R = Reduction (the amount to be deducted for noncompliance for a particular item/Defect/deficiency

The total amount of reduction shall be arrived at by summation of reductions for such items/Defects/deficiency or noncompliance.

For any Defect in a part of one kilometer, the non-conforming length shall be taken as one kilometer.

## **SCHEDULE-N**

(See Clause 18.1.1)

### **SELECTION OF AUTHORITY'S ENGINEER**

#### **1 Selection of Authority's Engineer**

- (i) The provisions of the Model Request for Proposal for Selection of Technical Consultants, issued by the Ministry of Finance in May 2009, or any substitute thereof shall apply for selection of an experienced firm to discharge the functions and duties of an Authority's Engineer.
  
- (ii) In the event of termination of the Technical Consultants appointed in accordance with the provisions of Paragraph 1.1, the Authority shall appoint another firm of Technical Consultants forthwith and may engage a government-owned entity in accordance with the provisions of Paragraph 3 of this Schedule-N.

#### **2 Terms of Reference**

The Terms of Reference for the Authority's Engineer (the "TOR") shall substantially conform with Annex 1 to this Schedule N.

#### **3 Appointment of Government entity as Authority's Engineer**

Notwithstanding anything to the contrary contained in this Schedule, the Authority may in its discretion appoint a government-owned entity as the Authority's Engineer; provided that such entity shall be a body corporate having as one of its primary functions the provision of consulting, advisory and supervisory services for engineering projects; provided further that a government-owned entity which is owned or controlled by the Authority shall not be eligible for appointment as Authority's Engineer.

Annex – I

(Schedule - N)

**TERMS OF REFERENCE FOR AUTHORITY’S ENGINEER**

**1. Scope**

(i) These Terms of Reference (the “**TOR**”) for the Authority’s Engineer are being specified pursuant to the EPC Agreement dated ..... (the “**Agreement**”), which has been entered into between the Ministry of Road Transport and Highways (the “**Authority**”) and ..... (the “**Contractor**”) for “Rehabilitation and up-gradation of section from Km 45.00 to 59.20 (After Ferrargunj to Jirkatang) of NH-4 to Intermediate lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands through Engineering, Procurement & Construction (EPC) Contract, and a copy of which is annexed hereto and marked as Annex-A to form part of this TOR.

(ii) The TOR shall apply to construction and maintenance of the Project Highway.

**2. Definitions and interpretation**

(i) The words and expressions beginning with or in capital letters and not defined herein but defined in the Agreement shall have, unless repugnant to the context, the meaning respectively assigned to them in the Agreement.

(ii) References to Articles, Clauses and Schedules in this TOR shall, except where the context otherwise requires, be deemed to be references to the Articles, Clauses and Schedules of the Agreement, and references to Paragraphs shall be deemed to be references to Paragraphs of this TOR.

(iii) The rules of interpretation stated in Clauses 1.2, 1.3 and 1.4 of the Agreement shall apply, *mutatis mutandis*, to this TOR.

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### **3. General**

- (i) The Authority's Engineer shall discharge its duties in a fair, impartial and efficient manner, consistent with the highest standards of professional integrity and Good Industry Practice.
- (ii) The Authority's Engineer shall perform the duties and exercise the authority in accordance with the provisions of this Agreement, but subject to obtaining prior written approval of the Authority before determining:
- (a) Any Time extension;
  - (b) Any additional cost to be paid by the Authority to the Contractor;
  - (c) The Termination Payment; or
  - (d) issuance of Completion Certificate or
  - (e) Any other matter which is not specified in (a), (b) or (c) above and which creates an obligation or liability on either Party for a sum exceeding Rs. 5,000,000 (Rs. fifty lakh).
- (iii) The Authority's Engineer shall submit regular periodic reports, at least once every month, to the Authority in respect of its duties and functions under this Agreement. Such reports shall be submitted by the Authority's Engineer within 10 (ten) days of the beginning of every month.
- (iv) The Authority's Engineer shall inform the Contractor of any delegation of its duties and responsibilities to its suitably qualified and experienced personnel; provided, however, that it shall not delegate the authority to refer any matter for the Authority's prior approval in accordance with the provisions of Clause 18.2.
- (v) The Authority's Engineer shall aid and advise the Authority on any proposal for Change of Scope under Article 13.
- (vi) In the event of any disagreement between the Parties regarding the meaning, scope and nature of Good Industry Practice, as set forth in any provision of the Agreement,

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the authority's Engineer shall specify such meaning, scope and nature by issuing a reasoned written statement relying on good industry practice and authentic literature.

#### **4 Construction Period**

(i) During the Construction Period, the Authority's Engineer shall review and approve the Drawings furnished by the Contractor along with supporting data, including the geo-technical and hydrological investigations, characteristics of materials from borrow areas and quarry sites, topographical surveys, and the recommendations of the Safety Consultant in accordance with the provisions of Clause 10.1 (vi). The Authority's Engineer shall complete such review and approval and send its observations to the Authority and the Contractor within 15 (fifteen) days of receipt of such Drawings; provided, however that in case of a Major Bridge or Structure, the aforesaid period of 15 (fifteen) days may be extended upto 30 (thirty) days. In particular, such comments shall specify the conformity or otherwise of such Drawings with the Scope of the Project and Specifications and Standards.

(ii) The Authority's Engineer shall review any revised Drawings sent to it by the Contractor and furnish its comments within 10 (ten) days of receiving such Drawings.

(iii) The Authority's Engineer shall review the Quality Assurance Plan submitted by the Contractor and shall convey its comments to the Contractor within a period of 21 (twenty-one) days stating the modifications, if any, required thereto.

(iv) The Authority's Engineer shall complete the review of the methodology proposed to be adopted by the Contractor for executing the Works, and convey its comments to the Contractor within a period of 10 (ten) days from the date of receipt of the proposed methodology from the Contractor.

(v) The Authority's Engineer shall grant written approval to the Contractor, where necessary, for interruption and diversion of the flow of traffic in the existing lane(s) of the Project Highway for purposes of maintenance during the Construction Period in accordance with the provisions of Clause 10.4.

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(vi) The Authority's Engineer shall review the monthly progress report furnished by the Contractor and send its comments thereon to the Authority and the Contractor within 7 (seven) days of receipt of such report.

(vii) The Authority's Engineer shall inspect the Construction Works and the Project Highway and shall submit a monthly Inspection Report bringing out the results of inspections and the remedial action taken by the Contractor in respect of Defects or deficiencies. In particular, the Authority's Engineer shall include in its Inspection Report, the compliance of the recommendations made by the Safety Consultant.

(viii) The Authority's Engineer shall conduct the pre-construction review of manufacturer's test reports and standard samples of manufactured Materials, and such other Materials as the Authority's Engineer may require.

(ix) For determining that the Works conform to Specifications and Standards, the Authority's Engineer shall require the Contractor to carry out, or cause to be carried out, tests at such time and frequency and in such manner as specified in the Agreement and in accordance with Good Industry Practice for quality assurance. For purposes of this Paragraph 4.9, the tests specified in the IRC Special Publication-11 (Handbook of Quality Control for Construction of Roads and Runways) and the Specifications for Road and Bridge Works issued by MORTH (the "Quality Control Manuals") or any modification/substitution thereof shall be deemed to be tests conforming to Good Industry Practice for quality assurance.

(x) The Authority's Engineer shall test check at least 50 (fifty) percent of the quantity or number of tests prescribed for each category or type of test for quality control by the Contractor.

(xi) The timing of tests referred to in Paragraph 4.9, and the criteria for acceptance/rejection of their results shall be determined by the Authority's Engineer in accordance with the Quality Control Manuals. The tests shall be undertaken on a random sample basis and shall be in addition to, and independent of, the tests that

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may be carried out by the Contractor for its own quality assurance in accordance with Good Industry Practice.

(xii) In the event that results of any tests conducted under Clause 11.10 establish any Defects or deficiencies in the Works, the Authority's Engineer shall require the Contractor to carry out remedial measures.

(xiii) The Authority's Engineer may instruct the Contractor to execute any work which is urgently required for the safety of the Project Highway, whether because of an accident, unforeseeable event or otherwise; provided that in case of any work required on account of a Force Majeure Event, the provisions of Clause 21.6 shall apply.

(xiv) In the event that the Contractor fails to achieve any of the Project Milestones, the Authority's Engineer shall undertake a review of the progress of construction and identify potential delays, if any. If the Authority's Engineer shall determine that completion of the Project Highway is not feasible within the time specified in the Agreement, it shall require the Contractor to indicate within 15 (fifteen) days the steps proposed to be taken to expedite progress, and the period within which the Project Completion Date shall be achieved. Upon receipt of a report from the Contractor, the Authority's Engineer shall review the same and send its comments to the Authority and the Contractor forthwith.

(xv) The Authority's Engineer shall obtain from the Contractor a copy of all the Contractor's quality control records and documents before the Completion Certificate is issued pursuant to Clause 12.4.

(xvi) Authority's Engineer may recommend to the Authority suspension of the whole or part of the Works if the work threatens the safety of the Users and pedestrians. After the Contractor has carried out remedial measure, the Authority's Engineer shall inspect such remedial measures forthwith and make a report to the Authority recommending whether or not the suspension hereunder may be revoked.

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(xvii) In the event that the Contractor carries out any remedial measures to secure the safety of suspended works and Users, and requires the Authority's Engineer to inspect such works, the Authority's Engineer shall inspect the suspended works within 3 (three) days of receiving such notice, and make a report to the Authority forthwith, recommending whether or not such suspension may be revoked by the Authority.

(xviii) The Authority's Engineer shall carry out, or cause to be carried out, all the Tests specified in Schedule-K and issue a Completion Certificate or Provisional Certificate, as the case may be. For carrying out its functions under this Paragraph 4.18 and all matters incidental thereto, the Authority's Engineer shall act under and in accordance with the provisions of Article 12 and Schedule-K.

## **5. Maintenance Period**

(i) The Authority's Engineer shall aid and advise the Contractor in the preparation of its monthly Maintenance Programme and for this purpose carry out a joint monthly inspection with the Contractor.

(ii) The Authority's Engineer shall undertake regular inspections, at least once every month, to evaluate compliance with the Maintenance Requirements and submit a Maintenance Inspection Report to the Authority and the Contractor.

(iii) The Authority's Engineer shall specify the tests, if any, that the Contractor shall carry out, or cause to be carried out, for the purpose of determining that the Project Highway is in conformity with the Maintenance Requirements. It shall monitor and review the results of such tests and the remedial measures, if any, taken by the Contractor in this behalf.

(iv) In respect of any defect or deficiency referred to in Paragraph 3 of Schedule- E, the Authority's Engineer shall, in conformity with Good Industry Practice, specify the permissible limit of deviation or deterioration with reference to the Specifications and Standards and shall also specify the time limit for repair or rectification of any deviation or deterioration beyond the permissible limit.

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(v) The Authority's Engineer shall examine the request of the Contractor for closure of any lane(s) of the Project Highway for undertaking maintenance/repair thereof, and shall grant permission with such modifications, as it may deem necessary, within 5 (five) days of receiving a request from the Contractor. Upon expiry of the permitted period of closure, the Authority's Engineer shall monitor the reopening of such lane(s), and in case of delay, determine the Damages payable by the Contractor to the Authority under Clause 14.5.

## **6 Determination of costs and time**

(i) The Authority's Engineer shall determine the costs, and/or their reasonableness, that are required to be determined by it under the Agreement.

(ii) The Authority's Engineer shall determine the period of Time Extension that is required to be determined by it under the Agreement.

(iii) The Authority's Engineer shall consult each Party in every case of determination in accordance with the provisions of Clause 18.5.

## **7. Payments**

(i) The Authority's Engineer shall withhold payments for the affected works for which the Contractor fails to revise and resubmit the Drawings to the Authority's Engineer in accordance with the provisions of Clause 10.2.4 (d).

(ii) Authority's Engineer shall -

(a) within 10 (ten) days of receipt of the Stage Payment Statement from the Contractor pursuant to Clause 19.4, determine the amount due to the Contractor and recommend the release of 90 (ninety) percent of the amount so determined as part payment, pending issue of the Interim Payment Certificate; and

(b) within 15 (fifteen) days of the receipt of the Stage Payment Statement referred to in Clause 19.4, deliver to the Authority and the Contractor an Interim Payment

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Certificate certifying the amount due and payable to the Contractor, after adjustments in accordance with the provisions of Clause 19.10.

(iii) The Authority's Engineer shall, within 15 (fifteen) days of receipt of the Monthly Maintenance Statement from the Contractor pursuant to Clause 19.6, verify the Contractor's monthly statement and certify the amount to be paid to the Contractor in accordance with the provisions of the Agreement.

(iv) The Authority's Engineer shall certify final payment within 30 (thirty) days of the receipt of the final payment statement of Maintenance in accordance with the provisions of Clause 19.16.

## **8. Other duties and functions**

The Authority's Engineer shall perform all other duties and functions as specified in the Agreement.

## **9 Miscellaneous**

(i) A copy of all communications, comments, instructions, Drawings or Documents sent by the Authority's Engineer to the Contractor pursuant to this TOR, and a copy of all the test results with comments of the Authority's Engineer thereon, shall be furnished by the Authority's Engineer to the Authority forthwith.

(ii) The Authority's Engineer shall retain at least one copy each of all Drawings and Documents received by it, including 'as-built' Drawings, and keep them in its safe custody.

(iii) Within 90 (ninety) days of the Project Completion Date, the Authority's Engineer shall obtain a complete set of as-built Drawings, in 2 (two) hard copies and in micro film form or in such other medium as may be acceptable to the Authority, reflecting the Project Highway as actually designed, engineered and constructed, including an as-built survey illustrating the layout of the Project Highway and setback lines, if any, of the buildings and structures forming part of Project Facilities; and shall hand them over to the Authority against receipt thereof.

(iv) The Authority's Engineer, if called upon by the Authority or the Contractor or both, shall mediate and assist the Parties in arriving at an amicable settlement of any Dispute between the Parties.

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(v) The Authority's Engineer shall inform the Authority and the Contractor of any event of Contractor's Default within one week of its occurrence.

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## **SCHEDULE - O**

*(See Clauses 19.4.1, 19.6.1, and 19.8.1)*

### **Forms of Payment Statements**

#### **1. Stage Payment Statement for Works**

The Stage Payment Statement for Works shall state:

- (a) The estimated amount for the Works executed in accordance with Clause 19.3.1 subsequent to the last claim;
- (b) Amounts reflecting adjustments in price for the aforesaid claim;
- (c) The estimated amount of each Change of Scope Order executed subsequent to the last claim;
- (d) Amounts reflecting adjustment in price, if any, for (c) above in accordance with the provisions of Clause 13.2.3 (a);
- (e) Total of (a), (b), (c) and (d) above;
- (f) Deductions:
  - (i) Any amount to be deducted in accordance with the provisions of the Agreement except taxes;
  - (ii) Any amount towards deduction of taxes; and
  - (iii) Total of (i) and (ii) above.
- (g) Net claim: (e) – (f) (iii);
- (h) The amounts received by the Contractor upto the last claim:
  - (i) For the Works executed (excluding Change of Scope orders);
  - (ii) For Change of Scope Orders, and
  - (iii) Taxes deducted

#### **2. Monthly Maintenance Payment Statement**

The monthly Statement for Maintenance Payment shall state:

- (a) the monthly payment admissible in accordance with the provisions of the agreement;
- (b) the deductions for maintenance work not done;
- (c) net payment for maintenance due, (a) minus (b);

- (d) amounts reflecting adjustments in price under Clause 19.12; and
- (e) amount towards deduction of taxes

### **3. Contractor's claim for Damages**

**Note:** The Contractor shall submit its claims in a form acceptable to the Authority.

## **SCHEDULE-P**

(See Clause 20.1)

### **INSURANCE**

#### **1. Insurance during Construction Period**

(i) The Contractor shall effect and maintain at its own cost, from the Appointed Date till the date of issue of the last Completion Certificate, the following insurances for any loss or damage occurring on account of Non Political Event of Force Majeure, malicious act, accidental damage, explosion, fire and terrorism:

(a) insurance of Works, Plant and Materials and an additional sum of [15 (fifteen)] per cent of such replacement cost to cover any additional costs of and incidental to the rectification of loss or damage including professional fees and the cost of demolishing and removing any part of the Works and of removing debris of whatsoever nature; and

(b) Insurance for the Contractor's equipment and Documents brought onto the Site by the Contractor, for a sum sufficient to provide for their replacement at the Site.

(ii) The insurance under paragraph 1.1 (a) and (b) above shall cover the authority and the Contractor against all loss or damage from whatsoever cause arising under paragraph 1.1 other than risks which are not insurable at commercial terms.

#### **2. Insurance for Contractor's Defects Liability**

The Contractor shall effect and maintain insurance cover of not less than 15% of the Contract Price for the Works from the date of issue of the Completion

Certificate until the end of the Defects Liability Period for any loss or damage for which the Contractor is liable and which arises from a cause occurring prior to the issue of the Completion Certificate. The Contractor shall also maintain other insurances for maximum sums as may be required under the Applicable Laws and in accordance with Good Industry Practice.

### **3. Insurance against injury to persons and damage to property**

(i) The Contractor shall insure against each Party's liability for any loss, damage, death or bodily injury which may occur to any physical property (except things insured under Paragraph 1 and 2 of this Schedule or to any person (except persons insured under Clause 20.9), which may arise out of the Contractor's performance of this agreement and occurring before the issue of the Performance Certificate. This insurance shall be for a limit per occurrence of not less than the amount stated below with no limit on the number of occurrences.

The insurance cover shall be as per the applicable laws of government and procedure in vogue.

(ii) The insurance shall be extended to cover liability for all loss and damage to the Authority's property arising out of the Contractor's performance of this Agreement excluding:

- (a) the Authority's right to have the construction works executed on, over, under, in or through any land, and to occupy this land for the Works; and
- (b) Damage which is and unavoidable result of the Contractor's obligations to execute the Works.

### **4. Insurance to be in joint names**

The insurance under paragraphs 1 to 3 above shall be in the joint names of the Contractor and the Authority.

# Schedule-Q

*(See Clause 14.10)*

## Tests on Completion of Maintenance Period

1. **Riding Quality test:**

Riding quality test: Riding quality of each lane of the carriageway shall be checked with the help of a calibrated bump integrator and the maximum permissible roughness for purposes of this Test shall be [2,200 (two thousand and two hundred only)] mm for each kilometre.

2. **Visual and physical test:**

**The Authority's Engineer shall conduct a visual and physical check of construction to determine that all works and equipment forming part thereof conform to the provisions of this Agreement. The physical tests shall include measurement of cracking, rutting, stripping and potholes and shall be as per the requirement of maintenance mentioned in Schedule-E.**

# Schedule-R

(See Clause 14.10)

## Taking Over Certificate

I, ..... (Name and designation of the Authority's Representative) under and in accordance with the Agreement dated ..... (the "**Agreement**"), for [construction of the \*\*\*\*section (km \*\* to km \*\*) of

\*\*\*\*] (the "**Project Highway**") on Engineering, Procurement and Construction (EPC) basis through .....(Name of Contractor), hereby certify that the Tests on completion of Maintenance Period in accordance with Article 14 of the Agreement have been successfully undertaken to determine compliance of the Project Highway with the provisions of the Agreement and I hereby certify that the Authority has taken over the Project highway from the Contractor on this day.....

SIGNED, SEALED AND DELIVERED

(Signature)

(Name and designation of Authority's Representative)

(Address)

Rehabilitation of section from Km 298.00 to Km 330.662 (Karala to Kalipur) of NH-04 to Intermediate lane with hard shoulder in the Union Territory of Andaman & Nicobar Islands (Total Length 32.36 km) on EPC Mode (Package-VIII)

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